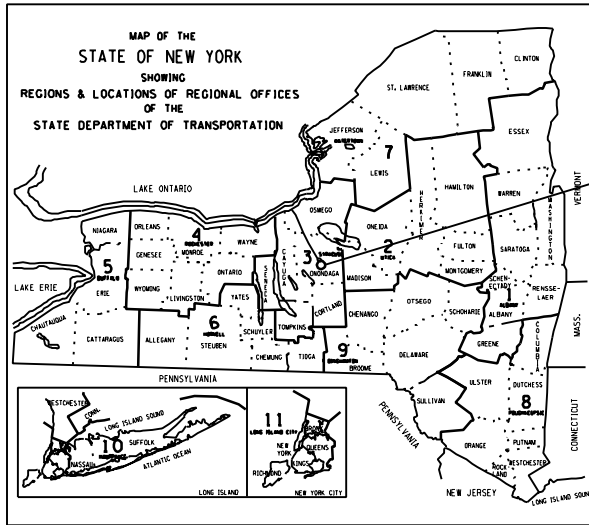
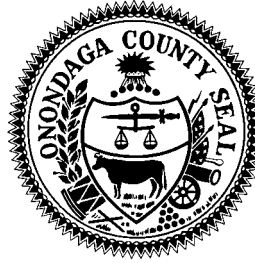


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 MCB CHECKED BY
 KAW CHECKED BY
 DESIGNED BY
 JPD
 IN CHARGE OF



PROJECT LOCATION



STANDARD SHEETS:
 ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY
 AND IN CONFORMITY WITH THE ONONDAGA COUNTY GREEN INFRASTRUCTURE
 PROGRAM 2015 ANNUAL GREEN STRUCTURES CONTRACT AT VARIOUS LOCATIONS.
 CONTRACT NUMBER: 1G - GENERAL; 1L - LANDSCAPE
 BID REFERENCE NUMBER: 7724
 VOLUME 11 OF 11; STANDARD DETAILS

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING
 UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER,
 ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN
 ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED
 PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT,
 LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE
 DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY
 THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC
 DESCRIPTION OF THE ALTERATION.

CSO 060/077 GREEN STREETS PROJECT ONONDAGA COUNTY DEPARTMENT OF WATER ENVIRONMENT PROTECTION

TASK ORDER: 2016-008G

CITY OF SYRACUSE
 ONONDAGA COUNTY, NY

OFFICE OF THE COUNTY EXECUTIVE
 JOANNE M. MAHONEY
 COUNTY EXECUTIVE
 OCDWEP COMMISSIONER
 TOM RHOADS

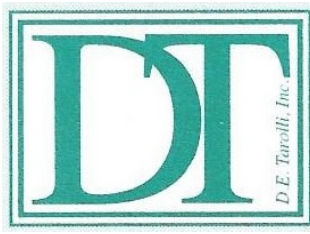
SEPTEMBER 2016

RECOMMENDED BY

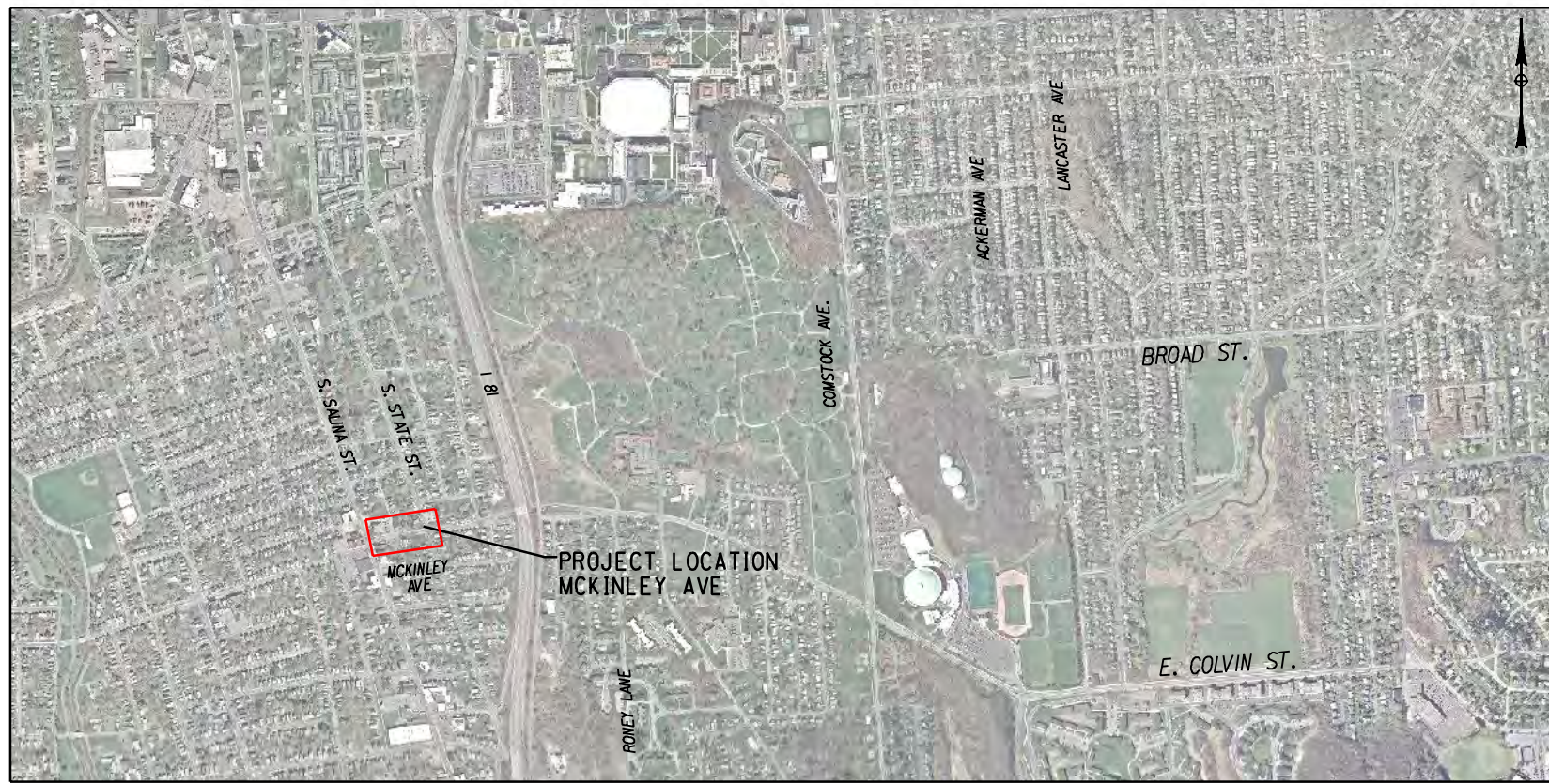
TOM RHOADS,
 OCDWEP COMMISSIONER

DATE

| DRAWING NAME | DRAWING # | SHEET # |
|--|------------------|---------|
| COVER | - | 1 |
| GENERAL NOTES AND LEGEND | GN-1 | 2 |
| MCKINLEY AVE DEMOLITION PLAN | DM-1 | 3 |
| DRAINAGE AREA PLAN | DA-1 | 4 |
| MCKINLEY AVE DRAINAGE TABLE | DRT-1 | 5 |
| MCKINLEY AVE INFILTRATION TRENCH PROFILE | PR-1 | 6 |
| MCKINLEY AVE GENERAL PLAN | MPL-1 | 7 |
| MISCELLANEOUS DETAILS | MD-1 | 8 |
| WORK ZONE TRAFFIC CONTROL | WZTC-1 TO WZTC-5 | 9 - 13 |



D.E. TAROLLI, INC.
 RECORD DRAWINGS
 11/30/2017



PROJECT LOCATION



PREPARED AND RECOMMENDED BY
 BARTON AND LOGUIDICE, D.P.C.

JOHN P. DONOHUE, P.E., LEED AP
 NEW YORK STATE PROFESSIONAL ENGINEERS LICENSE NO. 067059

DATE

CONTRACTOR'S NAME _____

AWARD DATE _____

COMPLETION DATE _____

FINAL ACCEPTANCE DATE _____

ENGINEER IN CHARGE _____

FINAL COST TOTAL _____

FISCAL SHARE _____ COST(S) _____

| | | | |
|--|-------|-----------|--------------|
| CSO 060/077 GREEN STREETS PROJECT | | | |
| TASK ORDER: 2016-008G | | | |
| ONONDAGA CO. DEPT. OF WATER ENVIRONMENT PROTECTION | | | |
| CITY OF SYRACUSE, ONONDAGA COUNTY | | | |
| FED. ROAD REG. NO. | STATE | SHEET NO. | TOTAL SHEETS |
| | N.Y. | 1 | 13 |
| FEDERAL AID PROJECT NO. | | | |
| CAPITAL PROJECT IDENTIFICATION NO. | | | |

GENERAL NOTES

STANDARD DETAIL REFERENCES IN THE PLANS REFER TO THE ONONDAGA COUNTY GREEN INFRASTRUCTURE 2015 ANNUAL GREEN STRUCTURES CONTRACT AT VARIOUS LOCATIONS, VOLUME II OF II: STANDARD DETAILS.

GENERAL CONSTRUCTION

EXISTING TOPOGRAPHY, STRUCTURES, AND SITE FEATURES ARE SHOWN SCREENED AND/OR LIGHT-LINED. NEW FINISH GRADE, STRUCTURES, AND SITE FEATURES ARE SHOWN HEAVY-LINED.

ALL DISTURBED SIGNS SHALL BE RE-INSTALLED IN KIND IN THE SAME LOCATION AS PER THE ONONDAGA COUNTY GREEN INFRASTRUCTURE PROGRAM 2016 ANNUAL GREEN STRUCTURES DETAIL TU.04, UNLESS OTHERWISE NOTED.

COORDINATES AND DIMENSIONS SHOWN FOR ROADWAY IMPROVEMENTS ARE TO FACE OF CURB OR EDGE OF PAVEMENT.

PROVIDE TEMPORARY FENCING AS NECESSARY TO MAINTAIN SAFETY AND SECURITY AT ALL TIMES. SEE THE GENERAL SPECIFICATIONS SECTION S-041.

ELEVATIONS GIVEN ARE TO FINSH GRADE UNLESS OTHERWISE NOTED.

SLOPE UNIFORMLY BETWEEN CONTOURS AND SPOT ELEVATIONS SHOWN.

UNLESS SHOWN, ALL DISTURBED AREAS NOT RECEIVING A HARD SURFACE SHALL BE RESTORED WITH GRASS AS SPECIFIED.

NOTIFY DIG SAFELY NEW YORK (811) AT LEAST 72 HOURS PRIOR TO CONSTRUCTION.

AGGREGATE FOR STORMWATER INFILTRATION TRENCHES SHALL BE CLEAN-WASHED PRIOR TO PLACEMENT.

MAINTAIN LOOSE AND UNDISTRUBED STORMWATER INFILTRATION TRENCH SUBGRADE. SCARIFY TOP 6 INCHES. DO NOT ALLOW CONSTRUCTION EQUIPMENT ON TRENCH BOTTOM SURFACE.

PROTECT EXISTING TREES IDENTIFIED ON THE PLANS TO REMAIN, DURING CONSTRUCTION.

UNLESS OTHERWISE NOTED, EXISTING GRANITE CURBING IS TO BE LEFT IN PLACE OR REPLACED TO THE NEAREST JOINT FROM THE DISTURBED AREA. WHERE DISTURBED DURING CONSTRUCTION, ALL EXISTING MANHOLE AND CATCH BASIN FRAMES AND COVERS SHALL BE REINSTALLED UPON A MINIMUM OF 2 COURSES OF BRICK, WHICH SHALL BE REPLACED IN KIND. THIS WORK SHALL BE PERFORMED AT THE CONTRACTOR'S EXPENSE.

CONTRACTOR SHALL PREPARE A TRAFFIC CONTROL PLAN FOR VEHICULAR AND PEDESTRIAN TRAFFIC IN ACCORDANCE WITH THE FEDERAL HIGHWAY ADMINISTRATION MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD). TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE CITY OF SYRACUSE PRIOR TO THE COMMENCEMENT OF ALL CONSTRUCTION ACTIVITIES. ALL TRAFFIC CONTROL DEVICES SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED FOR THE DURATION OF THE WORK.

WHERE DISTURBED, CONCRETE SIDEWALK TO BE REPLACED IN FULL WIDTH TO CLOSEST JOINT FROM DISTURBED AREA.

ADJUST TOPS OF EXISTING DRAINAGE STRUCTURES IN ACCORDANCE WITH NYSOT STANDARD SHEET 604-2.

AT CLOSE OF PROJECT, RECORD DRAWINGS VERIFYING ALL STRUCTURES ELEVATIONS SHALL BE COMPLETED BY A NYS LICENSED SURVEYOR.

AT CLOSE OF PROJECT, CONTRACTOR SHALL VACUUM ALL STRUCTURES CONNECTED TO AND WITHIN PROJECT AREA.

ALL WATER VALVE BOXES, SEWER CLEANOUTS, AND SEWER VENTS SHALL BE RESET TO MATCH NEW GRADES.

INSPECT AND CLEAN ALL EXISTING CATCH BASIN LATERALS PRIOR TO CONNECTING OVERFLOWS. CONNECTION SHALL BE MADE BY AN ONONDAGA COUNTY LICENSED PLUMBER AND REQUIRES A PLUMBING PERMIT THROUGH ONONDAGA COUNTY.

IF A LATERAL IS FOUND DAMAGED, CONTACT THE ENGINEER IMMEDIATELY. ONONDAGA COUNTY PLUMBING TO VERIFY WITH A PHOTO OR INSPECT. REPAIR SHALL OCCUR UNDER SEPARATE CHANGE ORDER.

IF FIRE HYDRANTS LEAKS ARE FOUND CONTACT THE ENGINEER AND SYRACUSE WATER DEPARTMENT IMMEDIATELY.

UNLESS OTHERWISE APPROVED BY THE CITY OF SYRACUSE, PEDESTRIAN AND VEHICULAR ACCESS TO WALKWAYS, BUSINESSES, AND DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES. SAFETY FENCING SHALL BE PER SECTION S-041 OF THE GENERAL SPECIFICATIONS.

EROSION AND SEDIMENT CONTROL

PROVIDE EROSION AND SEDIMENT CONTROL MEASURES WHERE AND WHEN APPROPRIATE AS PER THE NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL (AUGUST 2005, OR LATEST EDITION). PROTECT EXISTING SITE FEATURES UNLESS OTHERWISE NOTED. CONSULT ENGINEER PRIOR TO INFILTRATION TRENCH INSTALLATION.

DURING CONSTRUCTION, NO WET OR FRESH CONCRETE SHALL BE ALLOWED TO ESCAPE INTO ANY WATERS, NOR SHALL WASHINGS FROM CONCRETE TRUCKS, MIXERS, OR OTHER DEVICES BE ALLOWED TO ENTER ANY WATERS OR SEWERS. CONCRETE WASHOUTS SHALL BE USED.

CONTRACTOR SHALL TAKE ALL OTHER MEASURES TO POSITIVELY PRECLUDE ERODED MATERIALS FROM LEAVING THE SITE. CONTRACTOR TO SUBMIT EROSION CONTROL PLAN.

EROSION CONTROLS MUST BE CONSTRUCTED, STABILIZED, AND FUNCTIONAL BEFORE SITE DISTURBANCE BEGINS WITHIN THE TRIBUTARY AREAS OF THOSE CONTROLS.

AFTER FINAL SITE STABILIZATION HAS BEEN ACHIEVED, TEMPORARY EROSION CONTROLS MUST BE REMOVED. AREAS DISTURBED DURING REMOVAL OF THE CONTROLS MUST BE STABILIZED IMMEDIATELY.

UNTIL THE SITE IS STABILIZED, ALL EROSION CONTROLS MUST BE MAINTAINED PROPERLY. MAINTENANCE MUST INCLUDE INSPECTIONS OF ALL EROSION CONTROLS AFTER EACH RUNOFF EVENT AND ON A WEEKLY BASIS. ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, RE-GRADING, RE-SEEDING, RE-MULCHING AND RE-NETTING, MUST BE PERFORMED IMMEDIATELY. IF EROSION CONTROLS FAIL TO PERFORM AS EXPECTED, REPLACEMENT CONTROLS, OR MODIFICATIONS OF THOSE INSTALLED WILL BE REQUIRED.

PROTECT INFILTRATION TRENCH SUBGRADE FROM SEDIMENT DEPOSITION AND/OR COMPACTION DURING CONSTRUCTION. DO NOT ALLOW CONSTRUCTION VEHICLES TO TREAD ON THE INFILTRATION BED SUBGRADE.

CONTRACTOR SHALL FOLLOW INFILTRATION TRENCH AND DRAINAGE CONTROL REQUIREMENTS AS SPECIFIED BY ENGINEER.

SURVEY

COORDINATES AND NORTH ORIENTATION SHOWN HEREON ARE REFERENCED TO THE NEW YORK STATE PLANE COORDINATE SYSTEM, CENTRAL ZONE, TRANSVERSE MERCATOR PROJECTION, NAD 83/96 2011 EPOCH 2010.00 USING GPS PROCEDURES AND THE NEW YORK STATE DOT CORS NETWORK.

ELEVATIONS SHOWN HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 USING GPS PROCEDURES.

UNDERGROUND UTILITIES SHOWN HEREON WERE PLOTTED FROM FIELD LOCATIONS, VISIBLE AT THE TIME OF SURVEY, AND UTILITY COMPANY RECORD DRAWINGS. THE LOCATIONS OF ALL UNDERGROUND UTILITIES SHOULD BE STAKED BY THE RESPECTIVE UTILITY COMPANY PRIOR TO ANY CONSTRUCTION.

THE LOCATIONS OF STREET RIGHT-OF-WAYS ARE BASED ON A BOUNDARY SURVEY PERFORMED BY FISHER ASSOCIATES ON SEPTEMBER 9, 2015, USING INFORMATION RESEARCHED AT THE CITY OF SYRACUSE SURVEYORS OFFICE. PROPERTY LINES SHOWN HEREON ARE APPROXIMATE AND PLOTTED FOR GRAPHICAL REPRESENTATION ONLY.

THE CITY OF SYRACUSE SURVEY MONUMENTS SHALL NOT BE DISTURBED OR DESTROYED. IF THERE IS A CONFLICT OR A SURVEY MONUMENT IS WITHIN FIVE FEET OF CONSTRUCTION, THE CITY OF SYRACUSE DEPARTMENT OF ENGINEERING MUST BE CONTACTED AT (315) 448-8207 BEFORE ANY DEMOLITION OR CONSTRUCTION BEGINS.

SITE PROTECTION NOTES

THE CONTRACTOR SHALL PROVIDE AND MAINTAIN AT ALL TIMES A SAFE AND ADEQUATE INGRESS AND EGRESS TO AND FROM ALL PRIVATE AND PUBLIC PLACES OF BUSINESS.

THE CONTRACTOR SHALL COORDINATE WITH ANY AND ALL CONTRACTORS PERFORMING WORK ON THIS OR IMMEDIATELY ADJACENT TO THIS JOB SITE.

THE CONTRACTOR SHALL AT THEIR OWN EXPENSE, RESTORE LAWNS, DRIVEWAYS, CULVERTS, FENCES, GUIDERAILS, SIGNS AND OTHER PUBLIC AND PRIVATE PROPERTY DAMAGED OR REMOVED TO AT LEAST AS GOOD A CONDITION AS BEFORE BEING DISRUPTED.

EXCAVATED SPOILS NOT DESIGNATED FOR USE ON SITE SHALL BE REMOVED AT THE END OF EACH WORK DAY.

ALL BACKFILL MATERIAL STORED ON SITE SHALL BE COVERED TO PREVENT DUST AND MOISTURE INCREASE.

ALL TRUCKS ENTERING AND LEAVING THE SITE SHALL BE COVERED BY LAW TO REDUCE DUST AND ODOR. ALL MATERIALS (HAZARDOUS) SHALL BE LOCKED IN APPROPRIATE STORAGE UNITS.

CONTRACTOR SHALL MAINTAIN A CLEAN WORK SITE AT ALL TIMES. AT THE END OF THE WORK DAY ALL EQUIPMENT AND MATERIALS SHALL BE STORED IN THE DESIGNATED STAGING AREA. ALL SOIL, DUST AND MUD SHALL BE REMOVED FROM THE PROJECT AREA AND OUTSIDE THE PROJECT AREA. AT THE END OF THE DAY, TIRES OF CONSTRUCTION VEHICLES SHALL BE CLEANED OF SOIL AND MUD BEFORE BEING ALLOWED ON CITY STREETS. ANY SOIL OR MUD DEPOSITED ON CITY STREETS BY CONSTRUCTION VEHICLES SHALL BE REMOVED IMMEDIATELY.

CONTRACTOR SHALL PRESERVE AND MAINTAIN ALL EXISTING FACILITIES INCLUDING SIDEWALKS, SURVEY MONUMENTS, LIGHTING, CURBING AND PAVEMENT WITHIN THE PROJECT LIMITS.

CONTRACTOR SHALL REMOVE, OR PROPERLY CONTAINERIZE UNNECESSARY CONSTRUCTION DEBRIS AT THE END OF EACH WORK DAY.

UTILITIES

THE APPROXIMATE LOCATION OF THE UTILITIES ARE SHOWN ON THE PLANS. THE CONTRACTOR SHALL VERIFY THE TRUE LOCATION BEFORE COMMENCING WORK, AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICTS OR DISCREPENCIES. BEFORE ANY PIPE IS PLACED, THE CONTRACTOR SHALL UNCOVER ALL UTILITIES AT PIPE CROSSINGS TO ENABLE THE ENGINEER TO VERIFY THE PROPOSED PIPE WITH GRADES SHOWN ON THE PLANS IS NOT OBSTRUCTED BY EXISTING UTILITIES. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS IN THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES ENCOUNTERED IN THIS WORK. WHERE NECESSARY, THE CONTRACTOR SHALL PROVIDE TIMBER, OR OTHER APPROVED MATERIALS AND SECURELY BRACE AND PROTECT THESE UTILITIES. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS IN THE CONTRACT.

WHEN UTILITIES ARE ENCOUNTERED WITHIN THE REQUIREMED STORMWATER FACILITY EXCAVATION, CONTRACTOR SHALL WRAP EXISTING UTILITIES WITH 40 MIL GEOMEMBRANE SHEETING AND SECURE WITH PVC TAPE AT 2 FT OC (MAX); OVERLAP SHEETING BY 2 FT (MIN.); EXTEND SHEETING 12 INCHES BEYOND BOTH ENDS OF STORMWATER FACILITY. OVERLAP GEOMEMBRANE SHEETING A MINIMUM OF 57*64 OF THE PIPE CIRCUMFERENCE AND ENSURE OVERLAP RUNS ALONG CROWN OF PIPE. SLEEVES AND CASINGS SHALL BE MOISTURE-FREE BEFORE SEALING.

DURING UTILITY RELOCATION WORK REQUIRED FOR THIS CONTRACT, THE CONTRACTOR SHALL COOPERATE IN EVERY WAY WITH THE UTILITY OWNER, AND WILL SCHEDULE WORK IN SUCH A WAY AS TO COMPLY WITH SHUTDOWN TIMES AND ANY OTHER REQUIREMENTS OF THE UTILITY OWNER. NO ADDITIONAL PAYMENTS WILL BE MADE FOR ANY COST INCURRED DUE TO COMPLYING WITH OTHERS REQUIREMENTS. SUCH COSTS WILL BE INCLUDED IN PRICES BID FOR VARIOUS ITEMS IN THE CONTRACT.

THE FOLLOWING UTILITIES MAY BE ENCOUNTERED IN THE FIELD:

ELECTRIC, NATIONAL GRID: DONALD AMBROSE - 315.440.2115 - DONALD.AMBROSE@NATIONALGRID.COM
GAS, NATIONAL GRID: KATIE AYLING - 315.428.3379 - KATHERINE.AYLING@NATIONALGRID.COM
WATER, CITY OF SYRACUSE: KIM KELCHNER - 315.473.2609 x238
SANITARY/STORM SEWER, OCDWEP: JAMIE ISGAR - 315.744.0892 - JAMIEISGAR@ONGOV.NET
TELEPHONE, VERIZON: JOHN CONSIDINE - JOHN.J.CONSIDINE@VERIZON.COM
CABLE TELEVISION, TIME WARNER: STEVEN HICKS - 315.634.6225 - STEVEN.HICKS@TWCABLE.COM
PLUMBING CONTROL, OCDWEP: JOHN WILLIAMS - 315.435.6614 - JOHN.WILLIAMS@ONGOV.NET
STORM SEWER/GI, OCDWEP: ADAM WOODBURN - 315.435.5402 - ADAM.WOODBURN@ONGOV.NET

THE DEGREE OF ACCURACY FOR ALL UNDERGROUND UTILITIES WITHIN THE PROJECT LIMITS IS QUALITY LEVEL C/D.

EXISTING CONDITIONS LEGEND

Table with 2 columns: Symbol and Description. Includes: PROPERTY LINE/LEASE PARCEL LINE, RIGHT-OF-WAY LINE, EASEMENT LINE, BUILDING LINE, FENCE LINE, EDGE OF WATER, STREAM OR DITCH, EDGE OF WOODS OR BRUSH, SANITARY SEWER LINE W/MANHOLE & C.O., CULVERTS, STORM SEWER LINE W/MH & CATCH BASIN, WATER LINE W/HYDRANT, VALVE & VAULT, ELECTRIC LINE W/PULLBOX, METER & MANHOLE, NATURAL GAS LINE W/METER & VALVE, OVERHEAD WIRES, ELECTRIC, TELEPHONE & CABLE LINE, UNDERGROUND TELEPHONE LINE, UNDERGROUND FIBER OPTIC LINE, HEATING LINE (STEAM), SIGNAL ARM/HEAD, SIGNAL POLE, PEDESTRIAN POLE & TRAFFIC PULL BOX, TRAFFIC CONTROL LINE, UTILITY POLE, GUY, LIGHT POLE & TOP MOUNT LIGHT, WETLAND FLAG, BORING LOCATION.

PREPARED BY: BARTON & LOGUIDICE, D.P.C.
ON:

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IN CHARGE OF: JPD
DESIGNED BY: KAW
CHECKED BY: MCB
DRAFTED BY: PJZ
CHECKED BY: KAW

NO. DATE BY REVISION

ONONDAGA COUNTY

Barton & Loguidice, D.P.C.
UNAUTHORIZED ALTERATION OR ADDITION TO THIS DRAWING IS A VIOLATION OF THE NEW YORK STATE EDUCATION LAW ARTICLE 145 SECTION 7209

ONONDAGA COUNTY GREEN INFRASTRUCTURE IMPROVEMENTS
ONONDAGA COUNTY DEPARTMENT OF WATER ENVIRONMENT PROTECTION
CITY OF SYRACUSE ONONDAGA COUNTY

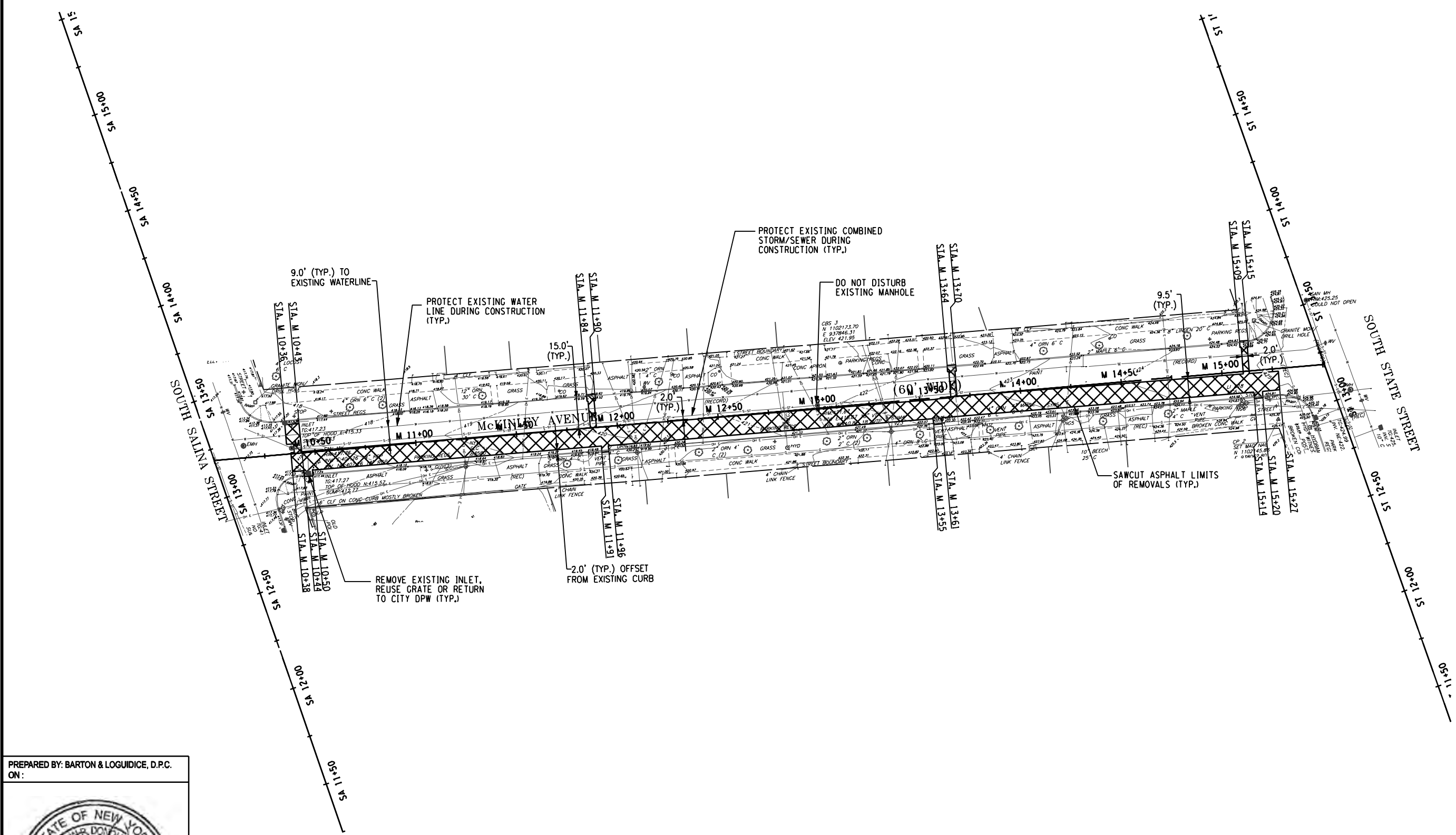
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PREPARED BY: BARTON & LOGUIDICE, D.P.C.
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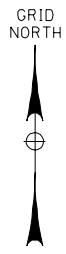


KEY:



NOTES:

- ALL EXISTING LOCAL AND STATE TRAFFIC SIGNAGE SHALL BE REMOVED AND RESET.
- CONCRETE WALK PAVEMENT REMOVALS SHALL BE IN AREAS INDICATED ON PLANS TO THE NEAREST CONTROL OR EXPANSION JT.



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Barton & Loguidice, D.P.C.

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ONONDAGA COUNTY
 GREEN INFRASTRUCTURE IMPROVEMENTS

ONONDAGA COUNTY DEPARTMENT OF
 WATER ENVIRONMENT PROTECTION

CITY OF SYRACUSE
 ONONDAGA COUNTY

**MCKINLEY AVE
 DEMOLITION PLAN**

SCALE: AS SHOWN
 DATE ISSUED: SEPT 2016
 DRAWING
 DM-1

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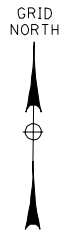
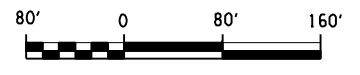


PREPARED BY: BARTON & LOGUIDICE, D.P.C.
 ON:

LEGEND

- DRAINAGE BASIN AREA
- M-#** MCKINLEY AVENUE DRAINAGE BASIN NUMBER
- ➔ DIRECTION OF SURFACE STORM WATER FLOW

| DRAINAGE BASIN AREA | |
|---------------------|----------|
| M1 | 1.83 AC. |
| M2 | 1.01 AC. |



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| <p>ONONDAGA COUNTY</p> | <p>NO. DATE BY REVISION</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 5%;"> </td> <td style="width: 15%;"> </td> <td style="width: 10%;"> </td> <td style="width: 10%;"> </td> <td style="width: 10%;"> </td> <td style="width: 10%;"> </td> <td style="width: 10%;"> </td> <td style="width: 10%;"> </td> </tr> </table> | | | | | | | | |
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| <p>Barton & Loguidice, D.P.C.</p> | <p>UNAUTHORIZED ALTERATION OR ADDITION TO THIS DRAWING IS A VIOLATION OF THE NEW YORK STATE EDUCATION LAW ARTICLE 145 SECTION 7209</p> | | | | | | | | |
| <p>ONONDAGA COUNTY GREEN INFRASTRUCTURE IMPROVEMENTS</p> <p>ONONDAGA COUNTY DEPARTMENT OF WATER ENVIRONMENT PROTECTION</p> <p>CITY OF SYRACUSE</p> <p>ONONDAGA COUNTY</p> | <p>MCKINLEY AVE DRAINAGE AREA</p> | | | | | | | | |
| <p>SCALE: AS SHOWN</p> <p>DATE ISSUED: SEPT 2016</p> <p>DRAWING DA-1</p> | | | | | | | | | |

IN CHARGE OF JPD DESIGNED BY KAW CHECKED BY MCB DRAFTED BY PJZ CHECKED BY KAW

| MCKINLEY DRAINAGE STRUCTURES (DOME OUTLETS & CATCH BASINS) | | | | | | | | | | |
|--|---------|------|--------|-----------|-----------|------------|--|--|--|------------|
| NO. | STATION | SIDE | OFFSET | SIZE/TYPE | RIM ELEV. | SUMP ELEV. | INV. ELEV. | PIPE INSTALLATION DESCRIPTION | | DETAIL NO. |
| DI M1-1 | M 10+46 | RT | 9' | A | 417.27 | 412.77 | N = 414.77 | N: INSTALL 5' OF SOLID HDPE PIPE AT 2.8% SLOPE, USE SOLID HDPE TEE TO TIE INTO MANHOLE M1-1 AT ELEV 414.63. | | DR-04 |
| MH M1-1 | M 10+46 | RT | 5' | - | 417.53 | 411.86 | N = 414.63 S = 414.63 E = 414.11 W = 413.86 | W (OVERFLOW CONNECTION): INSTALL 9' OF 8" SOLID HDPE PIPE AT 2.0% SLOPE TO EXISTING STORM MANHOLE. WEIR AT ELEV 415.36, LOW FLOW ORIFICE AT ELEV 413.86. N: INSTALL 21' OF 8" SOLID HDPE PIPE AT 1.5% SLOPE, USE SOLID HDPE TEE TO TIE INTO EXISTING CATCH BASIN AT ELEV 414.95. INSTALL ANTI-SEEP COLLAR WHERE PIPE LEAVES INFILTRATION TRENCH. | | DR-07 |
| DI M1-2 | M 11+88 | LT | 14' | A | 419.60 | 414.60 | S = 416.60 | S: INSTALL 19' OF SOLID 8" HDPE PIPE AT 6.0% SLOPE, USE SOLID HDPE TEE TO TIE INTO INFILTRATION TRENCH UNDERDRAIN AT ELEV 415.44. | | DR-04 |
| DI M1-3 | M 11+93 | RT | 10' | A | 419.64 | 413.48 | N = 415.48 | N: INSTALL 5' OF SOLID HDPE PIPE AT 8.0% SLOPE, USE SOLID HDPE TEE TO TIE INTO INFILTRATION TRENCH UNDERDRAIN AT ELEV 415.44. | | DR-04 |
| DI M1-4 | M 13+58 | RT | 10' | A | 422.13 | 415.40 | N = 417.40 | N: INSTALL 5' OF SOLID HDPE PIPE AT 10.0% SLOPE, USE SOLID HDPE TEE TO TIE INTO INFILTRATION TRENCH UNDERDRAIN AT ELEV 416.90. | | DR-04 |
| DI M1-5 | M 13+67 | LT | 13' | A | 422.21 | 416.70 | S = 418.70 | S: INSTALL 19' OF SOLID HDPE PIPE AT 9.5% SLOPE, USE SOLID HDPE TEE TO TIE INTO INFILTRATION TRENCH UNDERDRAIN AT ELEV 416.90. | | DR-04 |
| DI M1-6 | M 15+12 | LT | 12' | A | 424.70 | 418.22 | S = 420.22 | S: INSTALL 19' OF SOLID HDPE PIPE AT 10.0% SLOPE, USE SOLID HDPE TEE TO TIE INTO INFILTRATION TRENCH UNDERDRAIN AT ELEV 418.32. | | DR-04 |
| DI M1-7 | M 15+17 | RT | 11' | A | 424.76 | 416.82 | N = 418.82 | N: INSTALL 5' OF SOLID HDPE PIPE AT 10.0% SLOPE, USE SOLID HDPE TEE TO TIE INTO INFILTRATION TRENCH UNDERDRAIN AT ELEV 418.32. | | DR-04 |

| LEGEND | | |
|--------|----|------------------------------|
| DI | DI | DRAINAGE INLET (CATCH BASIN) |
| MX-N | X | PLAN DRAWING NUMBER |
| | N | STRUCTURE NUMBER |

PREPARED BY: BARTON & LOGUIDICE, D.P.C.
 ON:



NOTES:

- 1) ALL STRUCTURES WITH A DIRECT CONNECTION TO THE COMBINED SEWER MUST HAVE HOODS.
- 2) ALL CATCH BASINS AND PVC RISER STRUCTURES SHALL HAVE FILTER INSERTS INSTALLED.
- 3) ORIFICE DIAMETER FOR ALL STRUCTURES IS 1 INCH. ALL ORIFICES ARE TO BE PROVIDED WITH REMOVABLE PLUGS.

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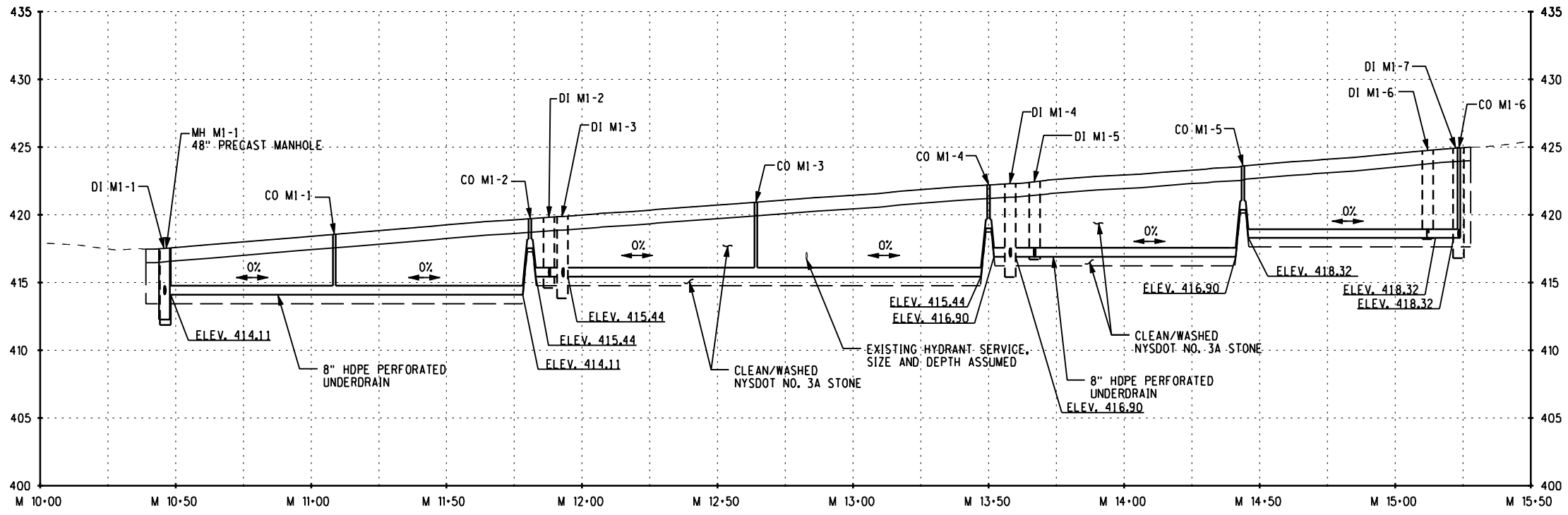
ONONDAGA COUNTY
 GREEN INFRASTRUCTURE IMPROVEMENTS
 ONONDAGA COUNTY DEPARTMENT OF WATER ENVIRONMENT PROTECTION
 CITY OF SYRACUSE
 ONONDAGA COUNTY

MCKINLEY AVE
 DRAINAGE
 TABLE
 SCALE: AS SHOWN
 DATE ISSUED: SEPT 2016
 DRAWING
 DRT-1

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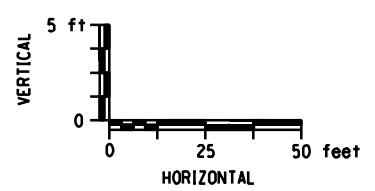
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IN CHARGE OF JPD DESIGNED BY KAW CHECKED BY MCB DRAFTED BY PJZ CHECKED BY KAW



INFILTRATION TRENCH PROFILE
 STA. M 10+38.00 TO STA. M 15+27.90; RT.

PREPARED BY: BARTON & LOGUIDICE, D.P.C.
 ON:



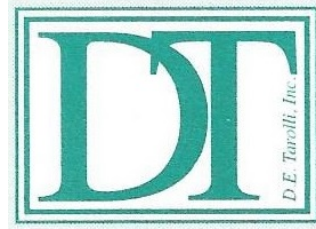
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MCKINLEY AVE
 INFILTRATION
 TRENCH PROFILE
 SCALE: AS SHOWN
 DATE ISSUED: SEPT 2016
 DRAWING
 PR-1



D.E. TAROLLI, INC. RECORD DRAWINGS 11/30/2017

GRID NORTH

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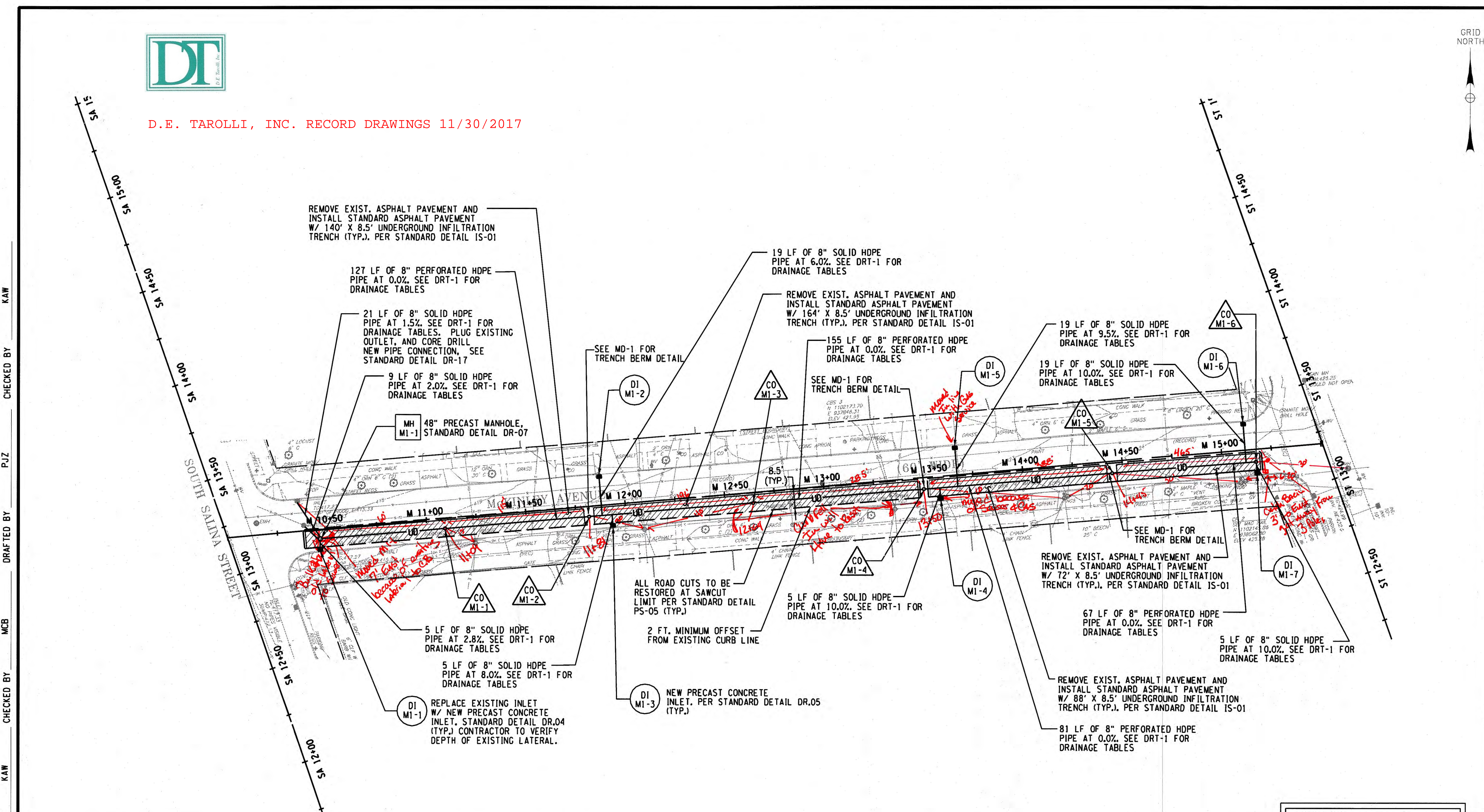
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Barton Loguidice, D.P.C.
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GREEN INFRASTRUCTURE IMPROVEMENTS
ONONDAGA COUNTY DEPARTMENT OF WATER ENVIRONMENT PROTECTION
CITY OF SYRACUSE
ONONDAGA COUNTY

MCKINLEY AVE
GENERAL PLAN
SCALE: AS SHOWN
DATE ISSUED: SEPT 2016
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IN CHARGE OF: JPD
DESIGNED BY: KAW
CHECKED BY: MCB
DRAFTED BY: PJZ
CHECKED BY: KAW



REMOVE EXIST. ASPHALT PAVEMENT AND INSTALL STANDARD ASPHALT PAVEMENT W/ 140' X 8.5' UNDERGROUND INFILTRATION TRENCH (TYP.). PER STANDARD DETAIL IS-01

127 LF OF 8" PERFORATED HDPE PIPE AT 0.0%. SEE DRT-1 FOR DRAINAGE TABLES

21 LF OF 8" SOLID HDPE PIPE AT 1.5%. SEE DRT-1 FOR DRAINAGE TABLES. PLUG EXISTING OUTLET, AND CORE DRILL NEW PIPE CONNECTION, SEE STANDARD DETAIL DR-17

9 LF OF 8" SOLID HDPE PIPE AT 2.0%. SEE DRT-1 FOR DRAINAGE TABLES

MH M1-1 48" PRECAST MANHOLE, STANDARD DETAIL DR-07

19 LF OF 8" SOLID HDPE PIPE AT 6.0%. SEE DRT-1 FOR DRAINAGE TABLES

REMOVE EXIST. ASPHALT PAVEMENT AND INSTALL STANDARD ASPHALT PAVEMENT W/ 164' X 8.5' UNDERGROUND INFILTRATION TRENCH (TYP.). PER STANDARD DETAIL IS-01

155 LF OF 8" PERFORATED HDPE PIPE AT 0.0%. SEE DRT-1 FOR DRAINAGE TABLES

19 LF OF 8" SOLID HDPE PIPE AT 9.5%. SEE DRT-1 FOR DRAINAGE TABLES

19 LF OF 8" SOLID HDPE PIPE AT 10.0%. SEE DRT-1 FOR DRAINAGE TABLES

19 LF OF 8" SOLID HDPE PIPE AT 10.0%. SEE DRT-1 FOR DRAINAGE TABLES

5 LF OF 8" SOLID HDPE PIPE AT 2.8%. SEE DRT-1 FOR DRAINAGE TABLES

5 LF OF 8" SOLID HDPE PIPE AT 8.0%. SEE DRT-1 FOR DRAINAGE TABLES

REPLACE EXISTING INLET W/ NEW PRECAST CONCRETE INLET, STANDARD DETAIL DR.04 (TYP.) CONTRACTOR TO VERIFY DEPTH OF EXISTING LATERAL.

ALL ROAD CUTS TO BE RESTORED AT SAWCUT LIMIT PER STANDARD DETAIL PS-05 (TYP.)

2 FT. MINIMUM OFFSET FROM EXISTING CURB LINE

NEW PRECAST CONCRETE INLET, PER STANDARD DETAIL DR.05 (TYP.)

5 LF OF 8" SOLID HDPE PIPE AT 10.0%. SEE DRT-1 FOR DRAINAGE TABLES

REMOVE EXIST. ASPHALT PAVEMENT AND INSTALL STANDARD ASPHALT PAVEMENT W/ 72' X 8.5' UNDERGROUND INFILTRATION TRENCH (TYP.). PER STANDARD DETAIL IS-01

67 LF OF 8" PERFORATED HDPE PIPE AT 0.0%. SEE DRT-1 FOR DRAINAGE TABLES

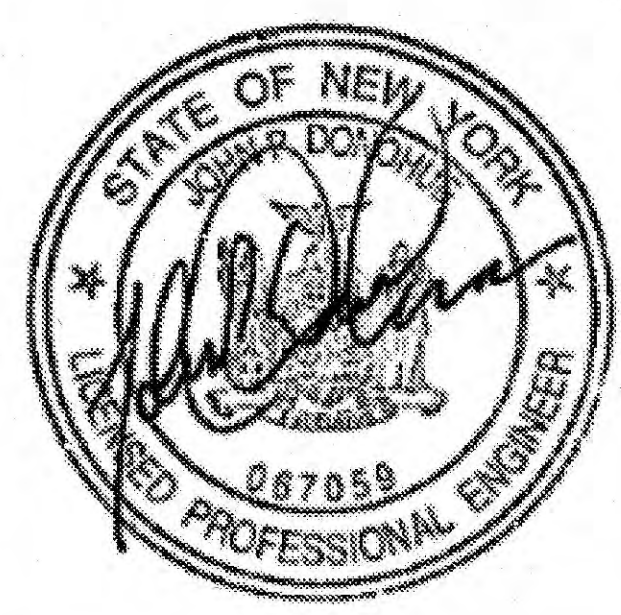
5 LF OF 8" SOLID HDPE PIPE AT 10.0%. SEE DRT-1 FOR DRAINAGE TABLES

REMOVE EXIST. ASPHALT PAVEMENT AND INSTALL STANDARD ASPHALT PAVEMENT W/ 88' X 8.5' UNDERGROUND INFILTRATION TRENCH (TYP.). PER STANDARD DETAIL IS-01

81 LF OF 8" PERFORATED HDPE PIPE AT 0.0%. SEE DRT-1 FOR DRAINAGE TABLES

| CLEANOUTS | | | | |
|-----------|---------|------|--------|------------|
| NO. | STATION | SIDE | OFFSET | DETAIL NO. |
| CO M1-1 | 11+09 | RT | 5' | DR-14 |
| CO M1-2 | 11+81 | RT | 5' | DR-14 |
| CO M1-3 | 12+64 | RT | 5' | DR-14 |
| CO M1-4 | 13+50 | RT | 6' | DR-14 |
| CO M1-5 | 14+45 | RT | 6' | DR-14 |
| CO M1-6 | 15+17 | RT | 6' | DR-14 |

NOTE: CONTRACTOR TO CONFIRM ELEVATION OF EXISTING WATER MAIN, CONTRACTOR TO NOTIFY ENGINEER OF ANY POTENTIAL CONFLICTS.



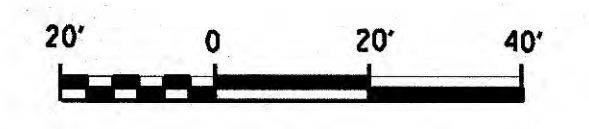
PREPARED BY: BARTON & LOGUIDICE, D.P.C.
ON: SEPTEMBER 2016

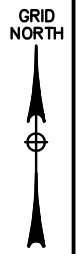
LEGEND:

- UNDERGROUND INFILTRATION TRENCH
- PROPOSED INLET (CB)
- EXISTING CB
- 8" PERF. HDPE UNDERDRAIN PIPE
- PAVEMENT SAWCUT LINE

KEY:

- DRAINAGE INLET
X - PLAN DRAWING NUMBER
N - STRUCTURE NUMBER
(SEE DRT-1 FOR GI TABLES)
- CLEANOUTS
X - PLAN DRAWING NUMBER
N - STRUCTURE NUMBER
(SEE DRT-1 FOR GI TABLES)
- MANHOLE
X - PLAN DRAWING NUMBER
N - STRUCTURE NUMBER
(SEE DRT-1 FOR GI TABLES)





NO. DATE BY REVISION



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ONONDAGA COUNTY
GREEN INFRASTRUCTURE IMPROVEMENTS
ONONDAGA COUNTY DEPARTMENT OF WATER ENVIRONMENT PROTECTION
CITY OF SYRACUSE
ONONDAGA COUNTY

MCKINLEY AVE GENERAL PLAN
SCALE: AS SHOWN
DATE ISSUED: SEPT 2018
DRAWING
MPL-1

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P.U.Z.
KAW
M.C.B.
KAW
J.P.D.

DESIGNED BY: MCB
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KAW
CHECKED BY: KAW
P.U.Z.
KAW

PREPARED BY: BARTON & LOGUIDICE, D.P.C.
ON: SEPTEMBER 2018

| CLEANOUTS | | | | |
|-----------|---------|------|--------|-------------|
| N.O. | STATION | SIDE | OFFSET | DETAIL N.O. |
| CO M1-1 | 11+09 | RT | 5' | DR-14 |
| CO M1-2 | 11+81 | RT | 5' | DR-14 |
| CO M1-3 | 12+64 | RT | 5' | DR-14 |
| CO M1-4 | 13+50 | RT | 6' | DR-14 |
| CO M1-5 | 14+45 | RT | 6' | DR-14 |
| CO M1-6 | 15+17 | RT | 6' | DR-14 |

LAND LINES

SURVEYING, P.C.

6181 JAMESVILLE TOLL ROAD
JAMESVILLE, NEW YORK 13078
315-492-4604

As-Built Survey

Colin M. Kraft

COLIN M. KRAFT LIC. No. 50450

DRAWN BY: WHJ

DATE: 12/20/2017

SCALE: 1"=50'

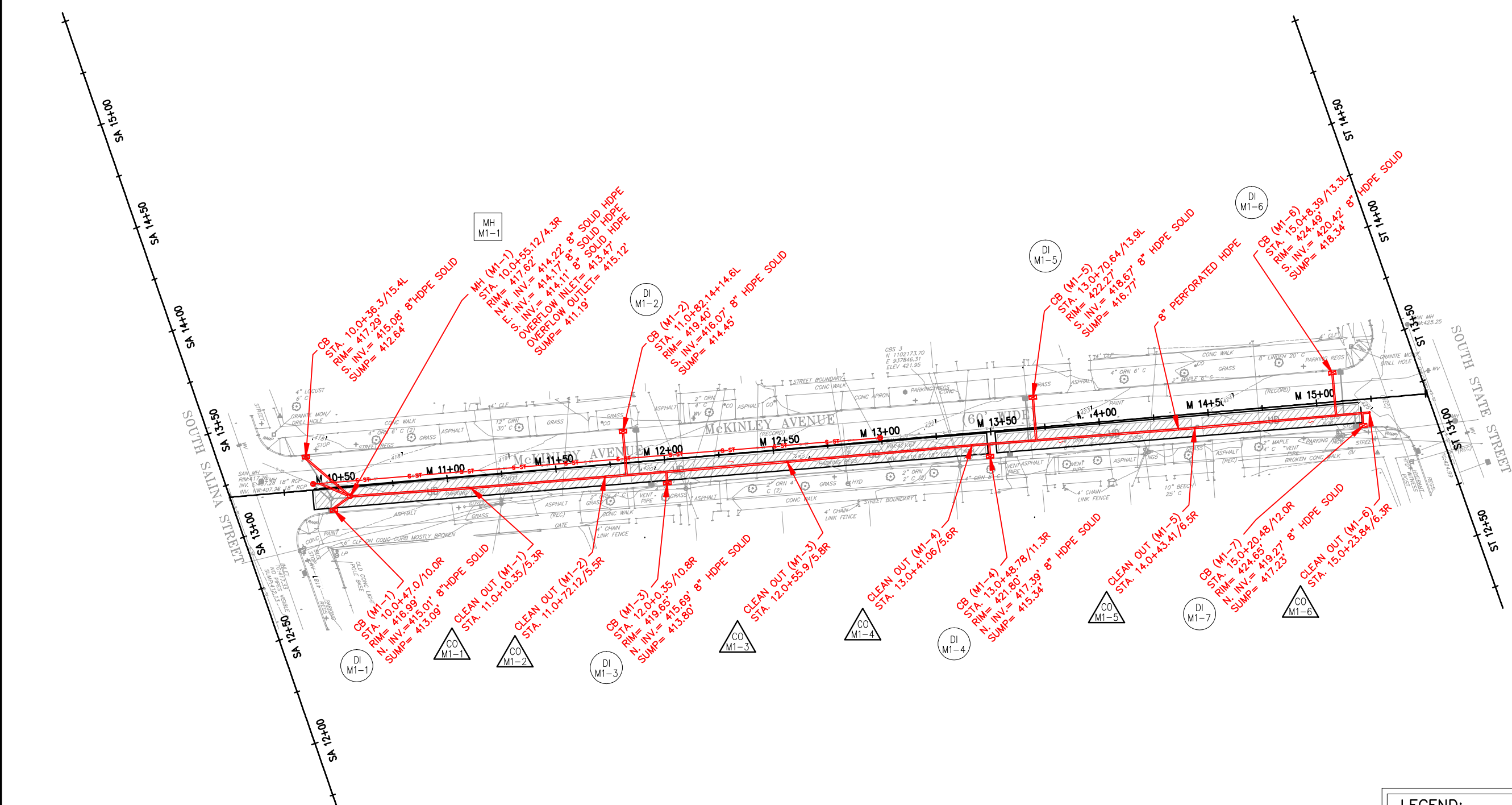
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KEY:

- DI MX-N - DRAINAGE INLET
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N = STRUCTURE NUMBER
(SEE DRT-1 FOR GI TABLES)
- CO MX-N - CLEANOUTS
X = PLAN DRAWING NUMBER
N = STRUCTURE NUMBER
(SEE DRT-1 FOR GI TABLES)
- MH MX-N - MANHOLE
X = PLAN DRAWING NUMBER
N = STRUCTURE NUMBER
(SEE DRT-1 FOR GI TABLES)

LEGEND:

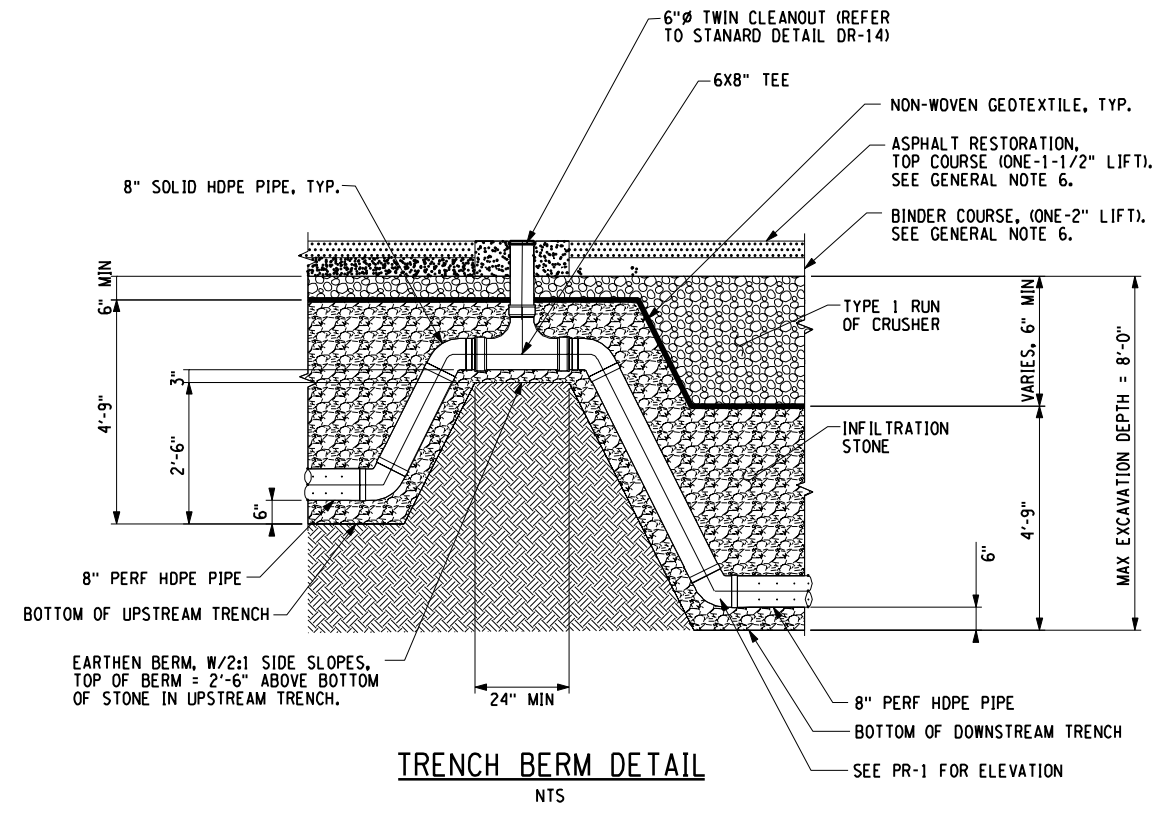
- UNDERGROUND INFILTRATION TRENCH
- PROPOSED INLET (CB)
- EXISTING CB
- 8" PERF. HDPE UNDERDRAIN PIPE
- PAVEMENT SAWCUT LINE



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IN CHARGE OF JPD DESIGNED BY KAW CHECKED BY MCB DRAFTED BY PJZ CHECKED BY KAW



TRENCH BERM DETAIL
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PREPARED BY: BARTON & LOGUIDICE, D.P.C.
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 CITY OF SYRACUSE
 ONONDAGA COUNTY

MISCELLANEOUS
 DETAILS
 SCALE: AS SHOWN
 DATE ISSUED: SEPT 2016
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WORK ZONE TRAFFIC CONTROL NOTES

1. GENERAL

- A. THE CONTRACTOR SHALL MAINTAIN TRAFFIC THROUGHOUT THE LENGTH AND DURATION OF THE CONTRACT IN ACCORDANCE WITH THE WORK TRAFFIC DETAILS IN THE PLANS AND PROPOSAL OR AOB.
- B. FOR TYPICAL APPLICATIONS OF TRAFFIC CONTROL DEVICES IN CONSTRUCTION AREAS NOT SPECIFIED IN THE PROPOSAL, THE PROVISIONS OF THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE NEW YORK STATE SUPPLEMENT, ALL APPLICABLE REVISIONS SHALL APPLY, THE STANDARDS OF APPLICATION NOTED THEREIN AND ON THE PROPOSAL ARE TO BE CONSIDERED MINIMAL STANDARDS. WHERE OPTIONS EXIST FOR SIGN SHAPE, THE DIAMOND SHAPE SHALL BE USED.
- C. PRIOR TO ANY WORK ZONE SET-UP THE ENGINEER SHALL DETERMINE THE ACCEPTABILITY OF ALL SIGNS, TRAFFIC CONTROL DEVICES AND WORK ZONE TRAFFIC CONTROL DEVICES.
- D. PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN SHALL BE SUBMITTED FOR THE REVIEW AND APPROVAL BY THE REGIONAL TRAFFIC ENGINEER 30 DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH REVISIONS.
- E. ANY AND ALL WORK RELATED TO WORK ZONE TRAFFIC CONTROL IS CONSIDERED SKILLED AND SPECIALIZED WORK. ALL WORKERS ENGAGED IN WORK ZONE TRAFFIC CONTROL WORK SHALL HAVE SUFFICIENT EXPERIENCE IN SUCH WORK TO SATISFACTORILY PERFORM IT. IDENTIFICATION OF EACH WORKER, ALONG WITH DOCUMENTATION OF EXPERIENCE, SHALL BE PROVIDED TO THE ENGINEER PRIOR TO THE WORKER ENGAGING IN WORK ZONE TRAFFIC CONTROL WORK. THE TRAFFIC CONTROL SUPERVISOR SHALL BE IN COMPLIANCE ACCORDING TO SECTION 619.3.20 OF THE NYS DOT STANDARD SPECIFICATION. DETERMINATION OF COMPETENCY AND DISCHARGING OF INDIVIDUALS SHALL BE AS PER SECTION 105-02 OF THE NYS DOT STANDARD SPECIFICATION.
- F. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THESE HIGHWAYS WILL BE DESIGNATED AS RESTRICTED UNDER THIS CONTRACT.

2. LANE CLOSURES

- A. THE CONTRACTOR SHALL SCHEDULE WORK SO THAT ALL TRAVEL LANES IN EACH DIRECTION ARE OPEN WHEN THE CONTRACTOR'S OPERATIONS ARE CLOSED DOWN OR SUBSTANTIALLY CLOSED DOWN. THE MINIMUM WIDTH OF A TRAVEL LANE SHALL BE 10.0 FT UNLESS OTHERWISE SHOWN IN THE PLANS OR PROPOSAL.
- B. IF, IN THE OPINION OF THE ENGINEER LANE CLOSURES ARE CREATING SIGNIFICANT DELAYS TO TRAFFIC OR ARE CREATING A SAFETY CONCERN DUE TO SLOWED OR STOPPED TRAFFIC, AFFECTED LANE CLOSURES SHALL BE REMOVED AND THE ROADWAY OPENED TO TRAFFIC AS DIRECTED BY THE ENGINEER.

3. WORK AREA COORDINATION

- A. WHEN TWO OR MORE WORK AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL COORDINATE ALL CONTRACT WORK WITH ANY, UTILITY WORK, SUBCONTRACTOR WORK, PUBLIC MAINTENANCE OPERATIONS OR OTHER CONSTRUCTION ACTIVITIES IN THE AREA TO ENSURE THAT THERE ARE NO WORK ZONE TRAFFIC CONTROL CONFLICTS, SIGNING AND LANE CONTINUITY, AND THAT WORK ZONE TRAFFIC CONTROL IS PER STANDARDS.
- B. ALL VEHICLES, EQUIPMENT, WORKERS AND ACTIVITIES SHALL BE RESTRICTED TO ONE SIDE OF THE ROADWAY AT A TIME, UNLESS OTHERWISE SHOWN IN THE PLANS OR PROPOSAL.
- C. THE CONTRACTOR SHALL COORDINATE ALL CONTRACT WORK WITH ANY ADJACENT PROJECTS TO ENSURE THERE ARE NO WORK ZONE TRAFFIC CONTROL CONFLICTS, SIGNING AND LANE CONTINUITY, AND THAT WORKZONE TRAFFIC CONTROL IS PER STANDARDS.

4. CONES, DRUMS, BARRICADES AND MARKERS

- A. ALL CHANNELIZING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM OF 2.0 FT MIN. CLEARANCE TO THE TRAVELED WAY UNLESS OTHERWISE SHOWN ON THE PLANS. WHERE POSSIBLE ALATERAL BUFFER SPACE OF 2.0 FT MIN. SHOULD BE PROVIDED BETWEEN THE WORK SPACE AND THE LINE OF DELINEATION DEVICES.
- B. LONGITUDINAL SPACING SHALL BE 40 FT FOR POSTED SPEED LIMITS OVER 40 MPH AND 20 FT FOR SPEED LIMITS 40 MPH AND UNDER. THROUGHOUT THE WORK ZONE, REDUCED SPACING MAY BE REQUIRED AS SHOWN IN THE PLANS, SPECIFICATIONS OR AOB. INTERSECTIONS AND DRIVEWAYS SHALL BE CHANNELIZED AT 5.0 FT SPACING.
- C. DRUMS OR VERTICAL PANELS (8" x 36" OR 12" x 24") SHALL BE USED DURING THE HOURS OF DARKNESS. "TYPE B" LIGHTS SHALL BE REQUIRED ON THE FIRST DEVICE AND ON THE FIRST DEVICE AFTER EACH INTERSECTION.

5. SIGNS

- A. BLANK COVERS USED TO COVER PORTIONS OF EXISTING SIGNS SHALL BE OF A COLOR AND REFLECTORIZED MATERIAL MATCHING THAT OF THE SIGN BEING PARTIALLY COVERED.
- B. WHERE SHOULDER WIDTHS ARE LIMITED AND SIGNS CAN NOT BE ERECTED BEYOND THE SHOULDER, TEMPORARY SIGNS MAY NEED TO BE MOUNTED ON CONCRETE MEDIAN BARRIER OR BRIDGE PARAPETS, ETC.. PRIOR TO ERECTING THOSE SIGNS THE CONTRACTOR SHALL PROVIDE MOUNTING DETAILS TO AND RECEIVE APPROVAL FROM THE ENGINEER. SIGNS SHALL NOT ENCR OACH MORE THAN 4" INTO SHOULDERS USED BY PEDESTRIANS OR BICYCLES.
- C. SIGNS SHALL BE MOUNTED AS SHOWN IN CHAPTER 6F OF THE MUTCD AND THE NEW YORK STATE SUPPLEMENT.
- 6. BUMPS
 - A. THE CONTRACTOR SHALL TAKE ALL ACTION AS DIRECTED BY THE ENGINEER, TO ELIMINATE BUMPS, WHICH ARE DEFINED AS, A PAVEMENT CONDITION CONSIDERED BY THE ENGINEER TO BE SUFFICIENTLY ABRUPT ENOUGH TO CAUSE CONSIDERABLE DISCOMFORT, CARGO SHIFTING, OR DEFLECTION OF A VEHICLE FROM ITS TRUE COURSE AT PREVAILING DRIVING SPEEDS. ONLY WHEN IT IS NOT POSSIBLE OR FEASIBLE IN THE OPINION OF THE ENGINEER TO ELIMINATE A BUMP, SHALL A BUMP BE ALLOWED TO REMAIN.
 - B. BUMPS SHALL BE RAMPED AT A MINIMUM RATE OF, SPEED LIMIT HORIZONTAL TO 1 VERTICAL, (IE; 55 HORIZONTAL TO 1 VERTICAL FOR 55 MPH).
 - C. WB-1 BUMP SIGNS SHALL BE PLACED ON ALL APPROACHES TO A BUMP OR DIP CAUSED BY CONSTRUCTION OPERATIONS (IE: ROAD PLATES, MILLED PAVEMENT TERMINATION, ETC...) PER TABLE NY2C-4. NUMEROUS BUMPS OR DIPS SHALL WARRANT THE USE OF W8-8 "ROUGH ROAD" SIGNS INSTEAD OF THE W8-1 SIGN. W8-1 SIGNS SHALL BE SUPPLEMENTED WITH W16-4 "NEXT XXX FEET" PANELS AND POSTED ON THE MAINLINE IN ADVANCE OF THE CONDITION AND REPEATED AT EACH INTERSECTION.
 - D. AN OM1-3 MARKER SHALL BE LOCATED RIGHT AT OR IMMEDIATELY IN ADVANCE OF THE BUMP AND SHALL BE USED IN CONJUNCTION WITH THE W8-1 SIGN, AND EQUIPPED WITH A "TYPE B" WARNING LIGHT.
- 7. PUBLIC INGRESS AND EGRESS
 - A. THE CONTRACTOR SHALL PROVIDE PROPERTY OWNERS WITH PROPER ACCESS AS SHOWN IN THE PLANS AND AOB, TO THEIR DRIVEWAYS AND SHALL MAINTAIN THEM THROUGH ALL PHASE WORK AREAS AND SHALL DELINEATE THEM BY MEANS OF SIGNS AND CHANNELIZING DEVICES AOB.
 - B. WHERE DIRECT ACCESS TO DRIVEWAYS IS NOT POSSIBLE DUE TO NECESSARY CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL PLAN ALTERNATE MEANS OF ACCESS AND SUBMIT SUCH PLANS TO THE ENGINEER FOR APPROVAL. PROPERTY OWNERS/OCCUPANTS SHALL HAVE 24 HOUR NOTICE OF ANY CHANGES.
 - C. WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE HIGHWAY WITHIN THE TEMPORARY TRAFFIC CONTROL ZONE, ADDITIONAL TRAFFIC CONTROL DEVICES SHALL BE ERECTED AND FLAGGERS POSTED AT APPROPRIATE LOCATIONS OR AOB.
 - 8. CONSTRUCTION INGRESS AND EGRESS
 - A. VEHICLES, MATERIALS AND/OR EQUIPMENT, INCLUDING OUT OF SERVICE SIGNS, SHALL NOT BE PARKED OR STORED WITHIN 30 FT OF A ROADWAY USED BY THE GENERAL PUBLIC, OR ANY OTHER TRAVERSABLE AREAS IN WHICH AN ERRANT VEHICLE MAY STRIKE THE OBJECT.
 - 9. EMERGENCY ACCESS
 - A. DURING WORK OPERATIONS ON OR NEAR SIDE ROADS AND DRIVEWAYS, THE CONTRACTOR SHALL MAINTAIN ACCESS TO THOSE SIDE ROADS AND DRIVEWAYS FOR EMERGENCY VEHICLES, AOB. EVEN WHEN THE FACILITY IS CLOSED TO NORMAL TRAFFIC.
 - 10. PEDESTRIANS
 - A. WHERE WALKWAYS EXIST AND PEDESTRIANS CAN BE SAFELY MAINTAINED ON THEM, THEN PEDESTRIAN TRAFFIC SHALL BE MAINTAINED ON THEM.
 - B. FOR INSTANCES OF VERY SHORT SHOULDER CLOSURES AND LOW VEHICLE AND OR PEDESTRIAN VOLUMES, IN LIEU OF IMPLEMENTING THE CHANNELIZED PEDESTRIAN WALKWAY AS SHOWN ON THE PLANS, A FLAGGER CAN BE POSTED AT THE BEGINNING OF THE SHOULDER CLOSURE. THE FLAGGER SHALL STOP ALL APPROACHING TRAFFIC AND KEEP SUCH TRAFFIC STOPPED TO ALLOW PEDESTRIANS TO PASS THE WORK AREA, ANOTHER FLAGGER WOULD BE POSTED TO ASSIST THE PEDESTRIANS IN THE TRAVERSING OF THE WORK ZONE. WHERE THE LENGTH OF THE SHOULDER CLOSURE, VOLUME OF PEDESTRIAN TRAFFIC, THE VOLUME OF VEHICULAR TRAFFIC OR OTHER FACTORS RENDER THIS TREATMENT INEFFECTIVE, AS DETERMINED BY THE ENGINEER, THE CHANNELIZED PEDESTRIAN WALKWAY SHALL BE IMPLEMENTED.
 - C. WHERE IT IS DETERMINED BY THE ENGINEER THAT THERE IS A LIKELYHOOD OF PEDESTRIANS LEAVING THE DEDICATED WALKWAY TO CROSS THE ACTIVE LANE OF TRAFFIC, THE CHANNELIZING DEVICES SHALL BE SUPPLEMENTED WITH A R9-30 "NO PEDESTRIAN CROSSING SYMBOL" SIGN TO RESTRICT THE PEDESTRIAN MOVEMENT.

PREPARED BY: BARTON & LOGUIDICE, D.P.C.
ON:



IN CHARGE OF JPD
 DESIGNED BY KAW
 CHECKED BY MCB
 DRAFTED BY PJZ
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Barton & Loguidice, D.P.C.

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ONONDAGA COUNTY
GREEN INFRASTRUCTURE IMPROVEMENTS

ONONDAGA COUNTY DEPARTMENT OF
WATER ENVIRONMENT PROTECTION

CITY OF SYRACUSE
ONONDAGA COUNTY

WORK ZONE
TRAFFIC CONTROL
NOTES

SCALE: AS SHOWN

DATE ISSUED: SEPT 2016

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TIME = 2:15:20 PM

IN CHARGE OF JPD DESIGNED BY KAW CHECKED BY MCB DRAFTED BY PJZ CHECKED BY KAW


| CLOSURE TYPE | EXPOSURE CONDITION ¹ | USE REQUIREMENTS ^{4,5} | | | |
|------------------|---|---------------------------------|---|-----------------------|-----------------------|
| | | FREEWAY | NON-FREEWAY (PRECONSTRUCTION POSTED SPEED LIMIT) | | |
| | | | w 45 MPH | 35-40 MPH | 1 30 MPH |
| LANE CLOSURE | WORKERS ON FOOT OR IN VEHICLES EXPOSED TO TRAFFIC | REQUIRED ³ | REQUIRED ³ | REQUIRED ³ | OPTIONAL ² |
| | NON-TRAVERSABLE HAZARD (IE. EQUIPMENT, MATERIALS, EXCAVATION) ONLY NO WORKERS EXPOSED | REQUIRED ³ | REQUIRED ³ | OPTIONAL ² | OPTIONAL ² |
| SHOULDER CLOSURE | WORKERS ON FOOT OR IN VEHICLES EXPOSED TO TRAFFIC | REQUIRED ³ | REQUIRED ³ | OPTIONAL ² | OPTIONAL ² |
| | NON-TRAVERSABLE HAZARD (IE. EQUIPMENT, MATERIALS, EXCAVATION) ONLY NO WORKERS EXPOSED | REQUIRED ³ | OPTIONAL ² | OPTIONAL ² | OPTIONAL ² |

1. THE EXPOSURE CONDITIONS DESCRIBED IN TABLE NY1-A ASSUMES THERE IS NO POSITIVE PROTECTION (TEMPORARY TRAFFIC BARRIER) PRESENT. WHERE WORKERS OR HAZARDS ARE PROTECTED BY A TEMPORARY TRAFFIC BARRIER, BARRIER VEHICLES ARE NOT REQUIRED.
2. WHERE THE REQUIREMENT IS "OPTIONAL", EITHER A BARRIER VEHICLE OR THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.
3. REQUIREMENTS SHALL INCLUDE PROVIDING A SEPARATE BARRIER VEHICLE FOR EACH CLOSED LANE AND EACH CLOSED PAVED SHOULDER 8' OR GREATER IN WIDTH. IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE BARRIER VEHICLE SHALL BE REPOSITIONED ACCORDINGLY. BARRIER VEHICLES PROTECTING NON-TRAVERSABLE HAZARDS SHALL REMAIN IN PLACE DURING BOTH WORKING AND NON-WORKING HOURS UNTIL THE HAZARD NO LONGER EXISTS. EXCEPTIONS TO THESE REQUIREMENTS MAY BE MADE, AS APPROVED BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE WHERE BARRIER VEHICLE PLACEMENT WOULD BE INEFFECTIVE OR WOULD INTERFERE WITH THE SAFE OPERATION OF TRAFFIC.
4. BARRIER VEHICLES ARE NOT REQUIRED FOR MILLING AND/OR PAVING OPERATIONS, BUT THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.
5. BARRIER VEHICLES ARE NOT REQUIRED FOR FLAGGING OPERATIONS, BUT THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.

| CLOSURE TYPE | EXPOSURE CONDITION | USE REQUIREMENTS | | | |
|------------------|---|-------------------------|---|-------------------------|-------------------------|
| | | FREEWAY | NON-FREEWAY (PRECONSTRUCTION POSTED SPEED LIMIT) | | |
| | | | w 45 MPH | 35-40 MPH | 1 30 MPH |
| LANE CLOSURE | WHEN ANY WORKER, VEHICLE, OR OTHER HAZARD IS EXPOSED TO TRAFFIC | REQUIRED ^{2,4} | REQUIRED ^{2,4} | REQUIRED ^{2,4} | REQUIRED ^{2,4} |
| SHOULDER CLOSURE | WHEN ANY WORKER, VEHICLE, OR OTHER HAZARD IS EXPOSED TO TRAFFIC | REQUIRED ^{2,4} | REQUIRED ^{2,4} | REQUIRED ^{2,4} | REQUIRED ^{2,4} |

1. A MOBILE CLOSURE SHALL BE USED FOR ANY WORK ACTIVITY THAT MOVES CONTINUOUSLY OR INTERMITTENTLY ALONG THE TRAVELED WAY OR SHOULDER SLOWER THAN THE PREVAILING SPEED OF TRAFFIC. CHANNELIZING DEVICES ARE NOT USED FOR MOBILE CLOSURES.
2. SHADOW VEHICLES SHALL BE EQUIPPED WITH AN APPROVED REAR MOUNTED ATTENUATOR (TRUCK MOUNTED OR TRAILER MOUNTED) FOR THE FOLLOWING MOBILE CLOSURES: LANE CLOSURES ON FREEWAYS, LANE CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION POSTED SPEED LIMIT OF 35 MPH OR MORE, SHOULDER CLOSURES ON FREEWAYS, AND SHOULDER CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION SPEED LIMIT OF 45 MPH OR MORE.
3. FOR MOBILE LANE CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION POSTED SPEED LIMIT OF 30 MPH OR LESS AND MOBILE SHOULDER CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION SPEED LIMIT OF 40 MPH OR LESS, SHADOW VEHICLES ARE NOT REQUIRED TO BE EQUIPPED WITH A REAR MOUNTED ATTENUATOR.
4. A SHADOW VEHICLE IS USED TO PROTECT EXPOSED WORKERS (ON FOOT OR IN A VEHICLE) AND SHALL BE REQUIRED FOR ALL MOBILE CLOSURES. SHADOW VEHICLE REQUIREMENTS SHALL INCLUDE PROVIDING A SEPARATE SHADOW VEHICLE FOR EACH CLOSED LANE AND EACH CLOSED PAVED SHOULDER 8' OR GREATER IN WIDTH.

PREPARED BY: BARTON & LOGUIDICE, D.P.C.
ON:



| SPEED LIMIT (S) (MPH) | TAPER LENGTH (L) (FT.) |
|--------------------------|---------------------------|
| (40 MPH) OR LESS | $L = WS^2 / 60$ |
| (45 MPH) OR MORE | $L = WS$ |

L = TAPER LENGTH
W = WIDTH OF OFFSET (FT.)
S = PRECONSTRUCTION POSTED SPEED LIMIT (MPH)

| LATERAL SHIFT OF TRAFFIC FLOW PATH | TEMPORARY TRAFFIC CONTROL ZONE POSTED SPEED LIMIT | | | | | | | | | |
|------------------------------------|---|----------|----------|----------|----------|----------|----------|----------|----------|----------|
| | (25 MPH) | (30 MPH) | (35 MPH) | (40 MPH) | (45 MPH) | (50 MPH) | (55 MPH) | (60 MPH) | (65 MPH) | (70 MPH) |
| 4 | 45 | 60 | 85 | 110 | 180 | 200 | 220 | 240 | 260 | 280 |
| 5 | 55 | 75 | 105 | 135 | 225 | 250 | 275 | 300 | 325 | 350 |
| 6 | 65 | 90 | 125 | 160 | 270 | 300 | 330 | 360 | 390 | 420 |
| 7 | 75 | 105 | 145 | 190 | 315 | 350 | 385 | 420 | 455 | 490 |
| 8 | 85 | 120 | 165 | 215 | 360 | 400 | 440 | 480 | 520 | 560 |
| 9 | 95 | 135 | 185 | 240 | 405 | 450 | 495 | 540 | 585 | 630 |
| 10 | 105 | 150 | 205 | 270 | 450 | 500 | 550 | 600 | 650 | 700 |
| 11 | 115 | 165 | 225 | 295 | 495 | 550 | 605 | 660 | 715 | 770 |
| 12 | 125 | 180 | 245 | 320 | 540 | 600 | 660 | 720 | 780 | 840 |

| PRECONSTRUCTION POSTED SPEED LIMIT (MPH) | DISTANCE |
|--|----------|
| 25 | 155 FT. |
| 30 | 200 FT. |
| 35 | 250 FT. |
| 40 | 305 FT. |
| 45 | 360 FT. |
| 50 | 425 FT. |
| 55 | 495 FT. |
| 60 | 570 FT. |
| 65 | 645 FT. |

| PRECONSTRUCTION POSTED SPEED LIMIT (MPH) | PLACEMENT DISTANCE (FT.) | | | |
|--|--------------------------|--------------|-------------------|--------------|
| | BARRIER VEHICLES* | | SHADOW VEHICLES** | |
| | (18000 LBS.) | (24000 LBS.) | (18000 LBS.) | (24000 LBS.) |
| > 55 | 100 FT. | 200 FT. | 100 FT. | 200 FT. |
| 45 - 55 | 100 FT. | 200 FT. | 85 FT. | 165 FT. |
| < 45 | 85 FT. | 165 FT. | 50 FT. | 100 FT. |

* AS DEFINED IN NYS DOT STANDARD SPECIFICATION 619:
BARRIER VEHICLE - VEHICLE USED FOR STATIONARY SHOULDER CLOSURES, LANE CLOSURES, AND OTHER STATIONARY WORK ZONES.
MINIMUM DISTANCE SHOWN REFLECTS THE ACTUAL ROLL AHEAD DISTANCE FROM MANUFACTURER.

| PRECONSTRUCTION POSTED SPEED LIMIT (MPH) | PLACEMENT DISTANCE (FT.) | | | |
|--|--------------------------|--------------|-------------------|--------------|
| | SHADOW VEHICLES** | | SHADOW VEHICLES** | |
| | (18000 LBS.) | (24000 LBS.) | (18000 LBS.) | (24000 LBS.) |
| > 55 | 230 FT. | 330 FT. | 180 FT. | 280 FT. |
| 45 - 55 | 180 FT. | 280 FT. | 150 FT. | 250 FT. |
| < 45 | 100 FT. | 200 FT. | 100 FT. | 200 FT. |

* AS DEFINED IN NYS DOT STANDARD SPECIFICATION 619:
SHADOW VEHICLE - VEHICLE USED FOR MOBILE OR SHORT DURATION WORK OPERATIONS.
MINIMUM DISTANCE SHOWN REFLECTS THE ACTUAL ROLL AHEAD DISTANCE FROM MANUFACTURER.

| TYPE OF POSITIVE BARRIER | POSTED SPEED LIMIT | | | | |
|--|--------------------|--------|--------|--------|--------|
| | 30 MPH | 40 MPH | 50 MPH | 55 MPH | 65 MPH |
| TEMPORARY CONCRETE BARRIER | 8:1 | 11:1 | 14:1 | 16:1 | 20:1 |
| BOX BEAM OR HEAVY POST CORRUGATED BEAM | 7:1 | 9:1 | 11:1 | 12:1 | 15:1 |

| ROAD TYPE | DISTANCE BETWEEN SIGNS | | | SIGN LEGEND | |
|----------------------|------------------------|---------|---------|-------------|----------|
| | A (FT.) | B (FT.) | C (FT.) | XX | YY |
| URBAN (1 30 MPH+) | 100 | 100 | 100 | AHEAD | AHEAD |
| URBAN (35-40 MPH+) | 200 | 200 | 200 | AHEAD | AHEAD |
| URBAN (w 45 MPH+) | 350 | 350 | 350 | 1000 FT. | AHEAD |
| RURAL | 500 | 500 | 500 | 1500 FT. | 1000 FT. |
| EXPRESSWAY / FREEWAY | 1000 | 1500 | 2640 | 1 MILE | 1/2 MILE |

* PRECONSTRUCTION POSTED SPEED LIMIT
URBAN: (MEETS MORE THAN 1 OF THE FOLLOWING CRITERIA) SIDEWALKS, BICYCLE USAGE, CURBING, CLOSED DRAINAGE SYSTEMS, DRIVEWAY DENSITIES GREATER THAN 24 DRIVEWAYS PER MILE, MINOR COMMERCIAL DRIVEWAY DENSITIES OF 10 DRIVEWAYS PER MILE OR GREATER, MAJOR COMMERCIAL DRIVEWAYS, NUMEROUS RIGHT OF WAY CONSTRAINTS, HIGH DENSITY OF CROSS STREETS, 85TH PERCENTILE SPEEDS OF 45 MPH OR LESS.
RURAL: ANY AREA NOT EXHIBITING MORE THAN ONE OF THE ABOVE CHARACTERISTICS.
EXPRESSWAY: DIVIDED HIGHWAYS FOR TRAFFIC WITH FULL OR PARTIAL CONTROL OF ACCESS AND GENERALLY WITH GRADE SEPARATIONS AT MAJOR CROSSROADS.
FREEWAYS/INTERSTATE: LOCAL OR INTER REGIONAL HIGH-SPEED, DIVIDED, HIGH-VOLUME FACILITIES WITH FULL OR PARTIAL CONTROL OF ACCESS.

WORK DURATION DEFINITIONS
LONG-TERM STATIONARY IS WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS.
INTERMEDIATE-TERM STATIONARY IS WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.
SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.
SHORT DURATION IS WORK THAT OCCUPIES A LOCATION UP TO 1 HOUR.
MOBILE IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY.

| TYPE OF TAPER | TAPER LENGTH (L) |
|---------------------------------|------------------|
| MERCING TAPER | L |
| SHIFTING TAPER | L/2 |
| SHOULDER TAPER | L/3 |
| ONE-LANE, TWO-WAY TRAFFIC TAPER | 100 FT. MAXIMUM |
| DOWNSTREAM TAPER | 100 FT. PER LANE |

| SYMBOL | DESCRIPTION |
|--------|---|
| | ARROW PANEL |
| | ARROW PANEL, CAUTION MODE |
| | ARROW PANEL TRAILER OR SUPPORT |
| | CHANGEABLE MESSAGE SIGN (Pvms) |
| | CHANNELIZING DEVICE |
| | CRASH CUSHION/TEMPORARY IMPACT ATTENUATOR |
| | DIRECTION OF TEMPORARY TRAFFIC DETOUR |
| | DIRECTION OF TRAFFIC |
| | FLAGGER |
| | FLAG TREE |
| | LUMINAIRE |
| | PAVEMENT MARKINGS THAT SHALL BE REMOVED FOR A LONG TERM PROJECT |
| | SIGN, TEMPORARY |
| | TEMPORARY BARRIER |
| | TEMPORARY BARRIER WITH WARNING LIGHTS |
| | TRAFFIC OR PEDESTRIAN SIGNAL |
| | TYPE III BARRICADE |
| | WARNING LIGHTS |
| | WORK SPACE |
| | WORK VEHICLE |
| | WORK VEHICLE WITH TRUCK MOUNTED ATTENUATOR |

NO. DATE BY REVISION



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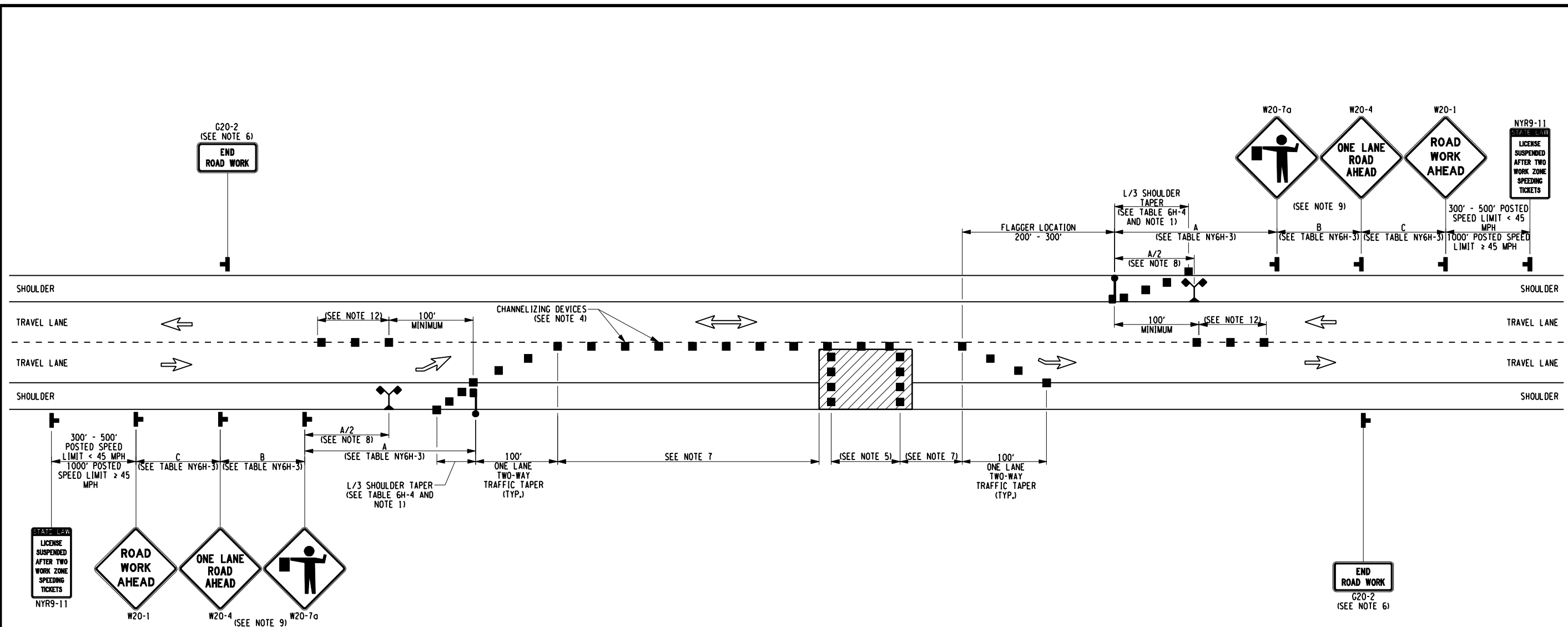
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ONONDAGA COUNTY DEPARTMENT OF WATER ENVIRONMENT PROTECTION
CITY OF SYRACUSE
ONONDAGA COUNTY

WORK ZONE TRAFFIC CONTROL TABLES AND SYMBOLS
SCALE: AS SHOWN
DATE ISSUED: SEPT 2016
DRAWING WZTC-2

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**FLAGGING OPERATION
 SHORT OR INTERMEDIATE TERM STATIONARY
 LANE CLOSURE ON 2-LANE 2-WAY ROADWAY
 (NOT TO SCALE)**

NOTES:

- WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK AREA AND DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVEL WAY.
- WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE ROADWAY WITHIN A WORK ZONE TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES AND/OR FLAGGERS SHALL BE PLACED AS NEEDED. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK SPACE.
- NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
- CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 40' IN THE ACTIVE WORK SPACE.
- TRANSVERSE DEVICES SHALL BE REQUIRED (AS PER 619 STANDARD SPECIFICATIONS) WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 1500'.
- THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 500' PAST THE END OF THE WORK SPACE.
- WHERE DIRECTED BY THE ENGINEER, A BUFFER SPACE SHALL BE PROVIDED IN ORDER TO LOCATE THE ONE-LANE, TWO-WAY TRAFFIC TAPER PRIOR TO ANY HORIZONTAL OR VERTICAL CURVE, IN ORDER TO PROVIDE ADEQUATE SIGHT DISTANCE FOR THE FLAGGERS AND/OR A QUEUE OF STOPPED VEHICLES.
- THE FLAG TREE SHALL BE LOCATED ON THE SHOULDER, AT APPROXIMATELY 1/2 THE DISTANCE BETWEEN THE FLAGGER SIGN (W20-7a) AND THE FLAGGER.
- FLAGGER SIGN (W20-7a) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED, COVERED OR TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
- FLAGGER AND FLAG TREE SHALL BE ILLUMINATED TO LEVEL II ILLUMINATION DURING NIGHT TIME OPERATIONS.
- ALL FLAGGERS SHALL USE 24" (MIN.) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6" STAFF.
- CENTERLINE CHANNELIZING DEVICES ARE OPTIONAL AND MAY BE ELIMINATED WHERE SPACE CONSTRAINTS EXIST.

| WORK ZONE TRAFFIC CONTROL LEGEND | |
|----------------------------------|----------------------|
| SYMBOL | DESCRIPTION |
| | DIRECTION OF TRAFFIC |
| | CHANNELIZING DEVICE |
| | FLAGGER |
| | FLAG TREE |
| | SIGN, TEMPORARY |
| | WORK SPACE |

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 ON:


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| CITY OF SYRACUSE ONONDAGA COUNTY | | | |
| FLAGGING OPERATION SINGLE LANE CLOSURE | | | |
| SCALE: AS SHOWN | | | |
| DATE ISSUED: SEPT 2016 | | | |
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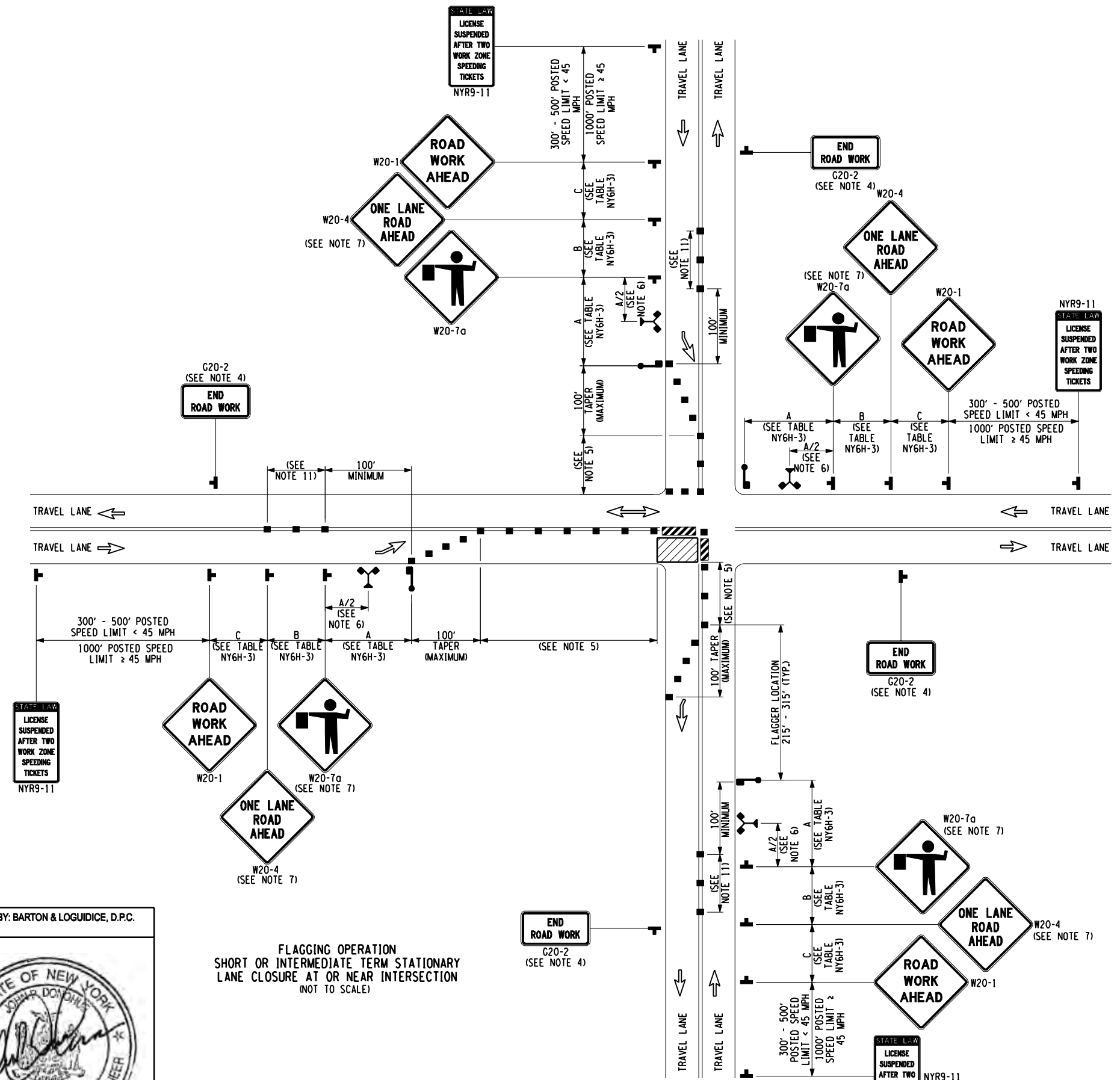
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PREPARED BY: BARTON & LOGUIDICE, D.P.C.
 ON:



**FLAGGING OPERATION
 SHORT OR INTERMEDIATE TERM STATIONARY
 LANE CLOSURE AT OR NEAR INTERSECTION
 (NOT TO SCALE)**



NOTES:

1. AT SIGNALIZED INTERSECTIONS, SIGNALS SHALL BE TURNED OFF FOR ANY FLAGGING OPERATIONS, UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
2. WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK AREA AND DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVEL WAY.
3. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 40' IN THE ACTIVE WORK SPACE.
4. THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 500' PAST THE END OF THE WORK SPACE.
5. WHERE DIRECTED BY THE ENGINEER, A BUFFER SPACE SHALL BE PROVIDED IN ORDER TO LOCATE THE ONE-LANE, TWO-WAY TRAFFIC TAPER PRIOR TO ANY HORIZONTAL OR VERTICAL CURVE, IN ORDER TO PROVIDE ADEQUATE SIGHT DISTANCE FOR THE FLAGGERS AND/OR A QUEUE OF STOPPED VEHICLES.
6. THE FLAG TREE SHALL BE LOCATED ON THE SHOULDER, AT APPROXIMATELY 1/2 THE DISTANCE BETWEEN THE FLAGGER SIGN (W20-7a) AND THE FLAGGER.
7. FLAGGER SIGN (W20-7a) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED, COVERED OR TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
8. FLAGGER AND FLAG TREE SHALL BE ILLUMINATED TO LEVEL II ILLUMINATION DURING NIGHT TIME OPERATIONS.
9. ALL FLAGGERS SHALL USE 24" (MIN.) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6' STAFF.
10. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK AREA.
11. CENTERLINE CHANNELIZING DEVICES ARE OPTIONAL AND MAY BE ELIMINATED WHERE SPACE CONSTRAINTS EXIST.
12. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
13. FOR PEDESTRIAN DETOUR ACCOMMODATIONS REFER TO THE STANDARD SHEETS TITLED "SIDEWALK DETOUR OR DIVERSION" AND "CROSSWALK CLOSURE AND PEDESTRIAN DETOUR".

| WORK ZONE TRAFFIC CONTROL LEGEND | |
|----------------------------------|----------------------|
| SYMBOL | DESCRIPTION |
| | DIRECTION OF TRAFFIC |
| | CHANNELIZING DEVICE |
| | FLAGGER |
| | FLAG TREE |
| | SIGN, TEMPORARY |
| | TYPE III BARRICADE |
| | WORK SPACE |

12
13

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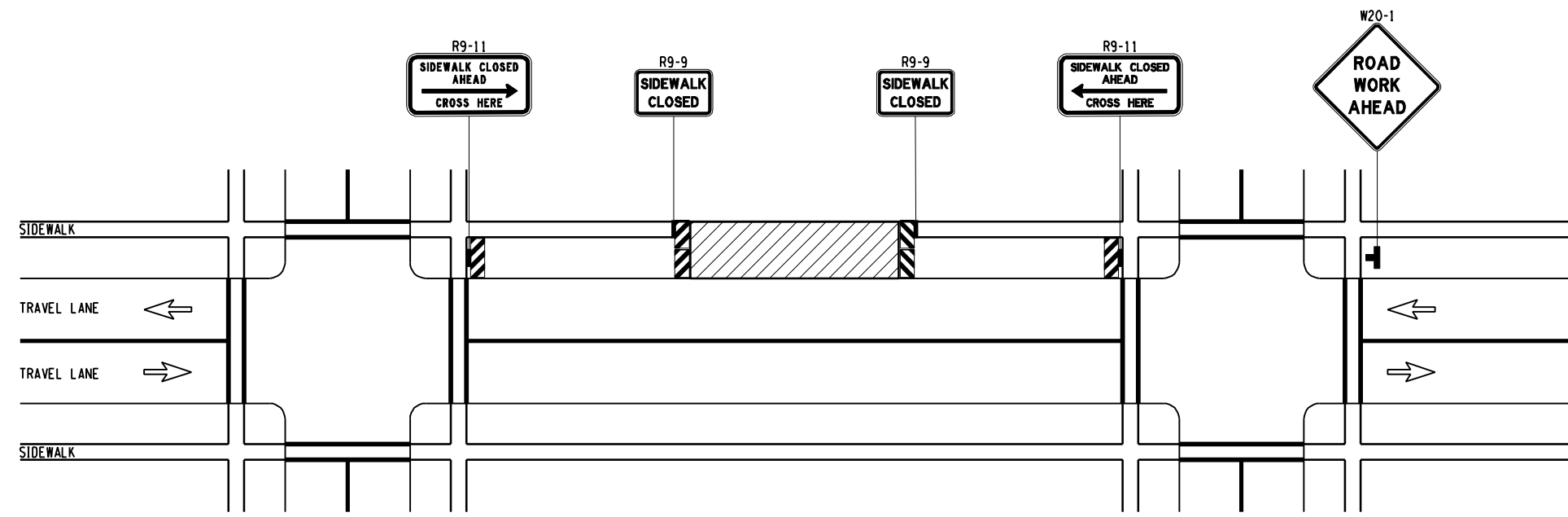
FLAGGING
 OPERATION
 LANE CLOSURE AT
 INTERSECTION
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 DATE ISSUED: SEPT 2016
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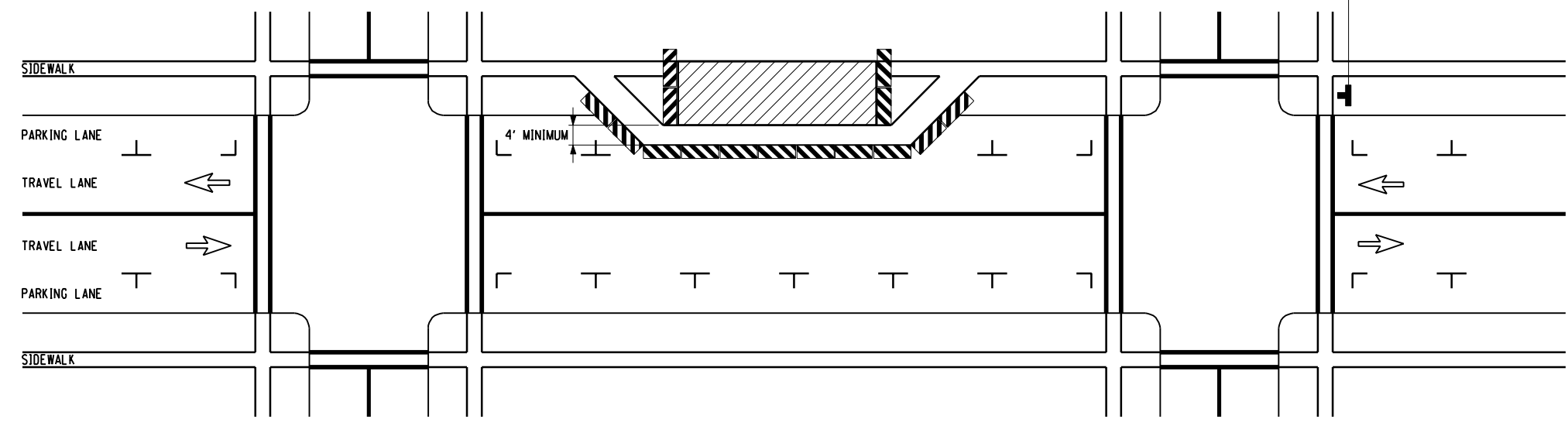
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 ON:



SIDEWALK DETOUR
 SHORT, INTERMEDIATE, OR LONG TERM STATIONARY
 2-LANE 2-WAY ROADWAY
 (NOT TO SCALE)



SIDEWALK DIVERSION
 SHORT, INTERMEDIATE, OR LONG TERM STATIONARY
 2-LANE 2-WAY ROADWAY
 (NOT TO SCALE)

| WORK ZONE TRAFFIC CONTROL LEGEND | |
|----------------------------------|----------------------|
| SYMBOL | DESCRIPTION |
| | DIRECTION OF TRAFFIC |
| | SIGN, TEMPORARY |
| | TYPE III BARRICADE |
| | WORK SPACE |

- NOTES:
- WHEN CROSSWALKS OR OTHER PEDESTRIAN FACILITIES ARE CLOSED OR RELOCATED, TEMPORARY FACILITIES SHALL BE DETECTABLE AND SHALL INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH THE FEATURES PRESENT IN THE EXISTING FACILITY.
 - WHERE HIGH SPEEDS ARE ANTICIPATED, A TEMPORARY TRAFFIC BARRIER AND TEMPORARY IMPACT ATTENUATOR SHOULD BE USED TO SEPARATE THE TEMPORARY SIDEWALKS FROM VEHICULAR TRAFFIC.
 - ONLY THE WORK ZONE TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES, SUCH AS LANE CLOSURE SIGNING OR ROAD NARROWS SIGNS (W5-4), MAY BE USED TO CONTROL VEHICULAR TRAFFIC.
 - FOR NIGHTTIME CLOSURES, FLASHING WARNING LIGHTS SHALL BE USED ON BARRICADES SUPPORTING SIGNS AND CLOSING SIDEWALKS.
 - SIGNS SUCH AS KEEP RIGHT (LEFT) SHALL BE PLACED ALONG A TEMPORARY SIDEWALK, WHERE APPLICABLE AND ACCORDING TO AMERICAN WITH DISABILITIES STANDARDS, TO GUIDE OR DIRECT PEDESTRIANS.
 - TYPE II BARRICADES MAY BE SUBSTITUTED FOR TYPE III BARRICADES AS PER 619 NYSDDT STANDARD SPECIFICATIONS.

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SIDEWALK DETOUR OR DIVERSION
 SCALE: AS SHOWN
 DATE ISSUED: SEPT 2016
 DRAWING WZTC-5